

APPLICATION FOR SITE PLAN CONTROL



PLANNING RATIONALE | 2500 St. Laurent Boulevard



Prepared for:

Conroy Business Park Inc.

1890 Broadmoor Avenue, Ottawa, ON K1H 5B4

Prepared by:

Lloyd Phillips & Associates Ltd.

File: 1715

Date: October 2, 2017



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1. OVERVIEW

Lloyd Phillips & Associates Ltd. have been retained by Conroy Business Park Incorporated (the 'Owner') to prepare a Planning Rationale report for a Site Plan Control Application with respect to the lands known municipally as 2500 St. Laurent Boulevard (the 'site'). The site is a vacant property located in the Ottawa Business Park, which accounts for a large employment area containing primarily business park and light industrial type uses.

The Owner's intent is to develop the site into a business park to be known as the Conroy Business Park Inc. The proposal does not require any amendments to the City's Official Plan or Zoning By-law, nor does it require an application to the Committee of Adjustment for minor variance. The intent is to develop the proposed business park in two separate phases, with the first accounting for the easterly three fourths of the property, and the second being the remaining westerly portion of the property. These phases would be subject to an application for a Phased Plan of Condominium, to be applied for following Site Plan Control Approval.

This report represents the required Planning Rationale component of the development application for Site Plan Control.



Figure 1. Parcel and phasing plan (2500 St. Laurent Boulevard outlined in red)

2. SITE AND SURROUNDING CONTEXT

2.1 Site

The site is a vacant corner lot bounded by Conroy Road to the east, St. Laurent Boulevard to the south, 2502 St. Laurent Boulevard (vacant lot) to the west, and 3000 Conroy Road (vacant lot) to the north. The site is rectangular in shape with its length and functional front yard along St. Laurent Boulevard (the site's legal front yard is along Conroy Road).

There are no buildings or structures that currently exist or that have historically existed on the site. The site's existing vegetation consists of grasses, shrubs and sparsely located trees along the property boundary and internal to the site. Until being purchased in 2016 by the current Owner, the site had been owned by the City of Ottawa since 2012. The site has remained vacant since the early 2000s following the closure of the Canadian Medical Laboratories Ltd. building which was located immediately north of the site at 3000 Conroy Road, and had been using the site as an overflow gravel parking lot (see Figure 2).

The site's Conroy Road frontage contains a series of north-south Hydro lines and a City-owned stormwater trunk sewer which is subject to an easement in favour of the City of Ottawa as set out in instrument OC1792917 (see the Surveyor's Real Property Report in Figure 3). Pedestrian routes exist along the Site's Conroy Road (multi-use pathway) and St. Laurent Boulevard frontage (sidewalk).

The site has a total area of 11,619.99 m² (1.16 ha) and has frontage of 123.17 metres on St. Laurent Boulevard and 87.86 metres on Conroy Road. The site's PIN is 04741-0095, and it is legally known as Part of Lot 1 Concession 4 (Rideau Front) Geographic Township of Gloucester City of Ottawa.



Figure 2. Aerial photo (2002) demonstrating the site's former use as a parking lot

SURVEYOR'S REAL PROPERTY REPORT

(PART 2)

**RE: 2500 ST. LAURENT BOULEVARD
Part of Lot 1
Concession 4 (Rideau Front)
Geographic Township of Gloucester
City of Ottawa**

1. Registered Rights-of-Way / Easements

Part 16, Plan 5R-11934, except Part 1 on Plan 4R-29431 is subject to an easement in favour of the City of Ottawa as set out in instrument OC1792917.

2. Property Improvements

No building structure is located on the subject property. This is a vacant land Surveyor's Real Property Report.

3. Compliance with Municipal Zoning Bylaws

Compliance is not certified by this report.

4. Additional Remarks

Refer to the plan to identify the survey bars that are on the actual corners or witness (WIT) the actual corners.

The area of the subject property is 1.1619 Hectares.

OTTAWA, Ontario
October 14, 2016
Our Reference 16656-16

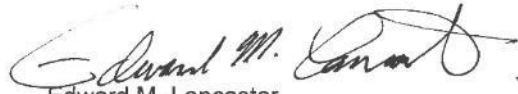

Edward M. Lancaster
Ontario Land Surveyor



Figure 3. Surveyor's Real Property Report

Figure 4. Survey Plan

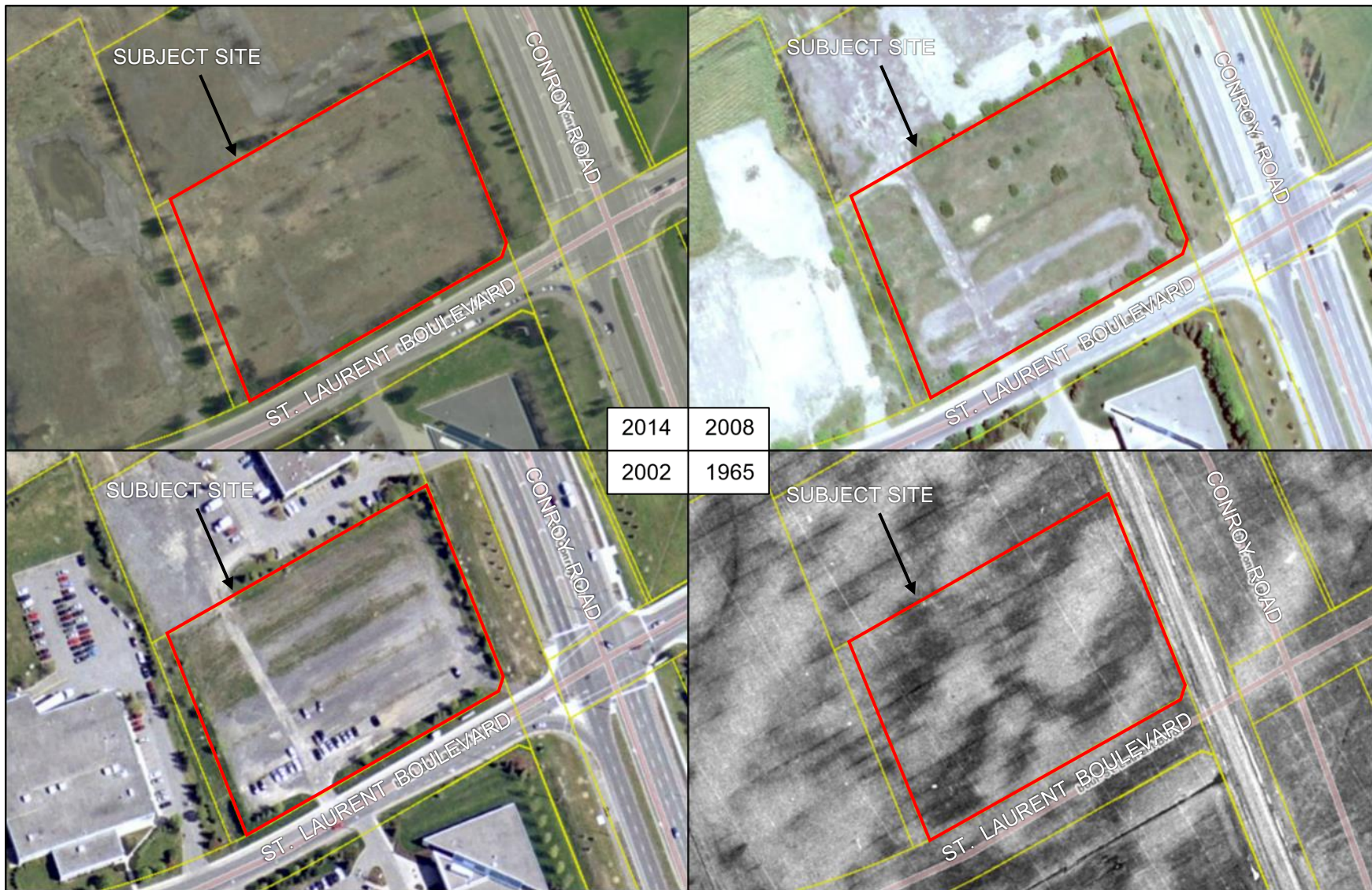


Figure 5. Historical air photos of the site

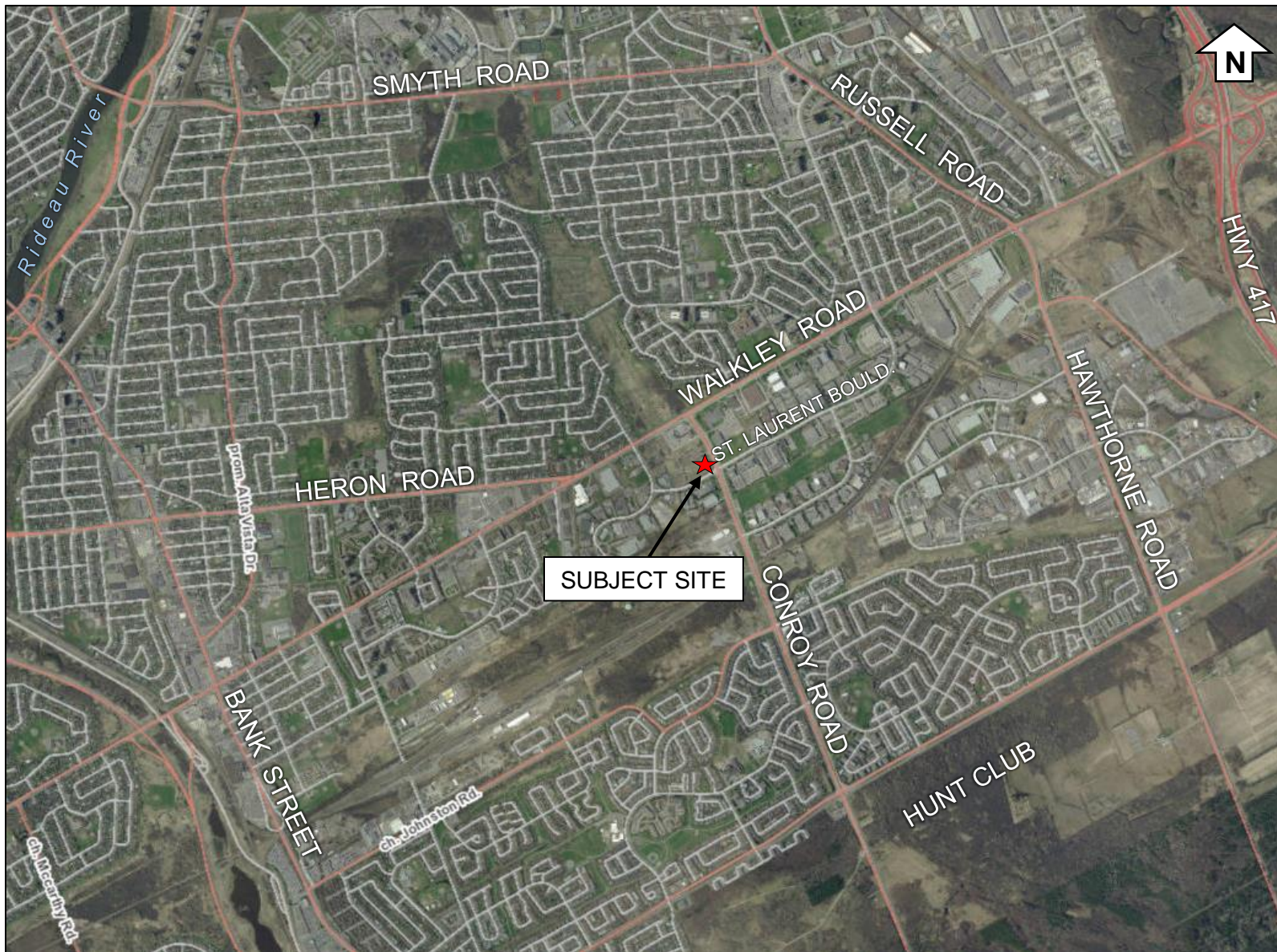


Figure 6. Location Plan



Figure 7. View facing south towards the site's easterly property line (note the Hydro lines)



Figure 8. View facing northwest towards the site's former entrance



Figure 9. View facing north along the site's southerly property line

2.2 Surrounding Context

The site is located in Ottawa South, within Ward 10 – Gloucester-Southgate. In a general context, the site is situated east of Bank Street, west of Highway 417, south of Walkley Road, and north of Hunt Club Road (see Figure 6).

With nearby access to major arterial roads, Highway 417, and a CN rail corridor, this area of Ottawa has historically acted as one of the City's largest collection of employment land. Much of the industrial type uses are located closer to Highway 417 and the rail corridor bounded by Johnston Road to the south. The business park uses of this large employment area tend to be centrally located between the two industrial areas.

More specifically, the site is located within a large land block that was designed for business park type uses along to the east (Conroy Road), south (St. Laurent Boulevard) and west (Don Reid Drive), with the intent to have commercial type uses along Walkley Road to the north.

The land uses and key features surrounding the site include the following (see Figure 10. Surrounding Uses Plan):

NORTH

- A large rectangular shaped vacant lot (former Canadian Medical Laboratories Ltd. building).
- Additional vacant lots are located to the north until reaching the intersection of Conroy Road and Walkley Road, which contains a Boston Pizza restaurant, an Esso gas bar, and two fast food restaurants (Tim Hortons and Wendy's).

EAST

- Conroy Road followed by a large business park block.
- The business park block contains a 24-hour Goodlife Fitness, a Sky Zone trampoline park, Airtron Canada, and a doctor's office.

SOUTH

- St. Laurent Boulevard followed by a chartered accountants office.
- Also, south of the site are three large lots containing multi-tenant office use buildings.

WEST

- A narrow vacant lot followed by a large rectangular shaped vacant lot.
- Following these lots is a property with a multi-tenant office use at the corner of St. Laurent Boulevard and Don Reid Drive.



Figure 10. Surrounding Uses Plan

3. PROPOSED DEVELOPMENT & DESIGN BRIEF

Building Design

Phase 1 will contain two linear two-storey buildings, with one building framing the easterly property line and the other framing the majority of the southerly property line. The southerly building will contain 10 separate condo units, while the easterly building will contain 8 separate condo units. The site's vehicle entrance will be along St. Laurent Boulevard and will represent the westerly limit of Phase 1. Phase 2, located on the western one fourth of the property, will consist of a north-south linear building of similarly designed office buildings (Figure 11 shows the proposed site plan).

The proposed linear buildings are designed to be well-articulated with well-defined recesses and vertical elements with projecting rooflines which function to provide a visual reduction in the perceived building length. The proposed elevations represent a high-level contemporary design that utilizes a combination of stone and brick which is compatible with the materiality of the existing business park. The proposed building windows are designed to be large and detailed in a varied and rhythmic pattern. The building signage and exterior lighting are an integral part of the exterior design, and have been incorporated into the site in a manner that is complementary to the buildings materiality and glazing, as well as the surrounding business park.

Table 1. Proposed developed attributes

TOTAL SITE AREA	USE	PHASE 1		PHASE 2
		BUILDING #1 GFA (Units 1-10)	BUILDING #2 GFA (Units 11-18)	Building / Unit Count TBD
11,619.99 m ²	Office	Ground Floor: 999.97 m ² Second Floor: 1,024.39 m ² Total: 2,024.36 m ²	Ground Floor: 783.01 m ² Second Floor: 802.49 m ² Total: 1,585.50 m ²	GFA TBD (±2,720 m ²)
		Phase 1 Total GFA: 3,609.86 m ²		

Vehicular Circulation and Parking

The site is located at the corner of Conroy Road and St. Laurent Boulevard. Conroy Road is designated as an urban arterial in the City's Official Plan and a 44.5 metre Right-of-Way (ROW) is to be protected. The portion of Conroy Road adjacent the site contains three northbound lanes and two southbound lanes, both of which include designated left turn lanes onto St. Laurent Boulevard, designated cycling lanes, and a large median separating opposing traffic flows. The southbound side of Conroy Road also contains an off-road multi-use pathway (MUP), while the northbound side contains a City-owned sidewalk.

The portion of St. Laurent Boulevard adjacent to the site is designated an urban collector in the City's Official Plan and therefore requires a ROW Protection of 26 metres. This portion of roadway contains a single set of east and westbound lanes with designated left and right turn lanes at the intersection with Conroy Road. The portion of the road abutting the site contains a City-owned sidewalk that, to

the east, connects to the MUP along Conroy Road, and to the west, extends to the intersection of St. Laurent Boulevard and Don Reid Drive.

There are no internal roadways or lanes currently existing on the site. The proposal will contain an internal ring road accessed from St. Laurent Boulevard, which will contain parking internal and external to the ring. The minimum amount of required vehicle parking for Phase 1 is 87 spaces, whereas, 141 spaces are to be provided. It is expected that the parking requirement for Phase 2 will be approximately 65 spaces (based upon an additional 2,720 m² of office use GFA), whereas 28 will be provided, resulting in a total site requirement of 161 spaces with 169 spaces provided.

The minimum amount of required bicycle parking for Phase 1 is 14 spaces, whereas, 20 spaces are to be provided. Based upon an additional 2,720 m² of office use GFA for Phase 2, the total requirement for bicycle parking would increase to 25 spaces, which would require that 5 additional spaces be provided.

The proposal requires a total of two accessible parking spaces and 2 regular loading spaces, both of which will be provided (one of each space provided in Phase 1 and the remaining two provided in Phase 2). See Table 3 for additional details regarding parking.

Pedestrian Connectivity and Landscaping

Pedestrian connectivity around the site is provided by City-owned sidewalks, a MUP along the west side of Conroy Road, and designated cycling lanes along the north and southbound lanes of Conroy Road. These types of pedestrian connections are ideal for the large lot and wide road allowance setting of the business park.

The intersection of Conroy Road and St. Laurent Boulevard is well-lit, provides depressed crosswalk curbs, and contains a wide median which separates the north and southbound traffic flows. With respect to public transit, four transit stops exist within 120 metres of the site, in locations easily accessible from the adjacent MUP, sidewalks, and crosswalks. These stops service the following buses: Bus No. 114 Greenboro; Bus No. 114 St-Laurent; Bus No. 644; and, Bus No. 649.

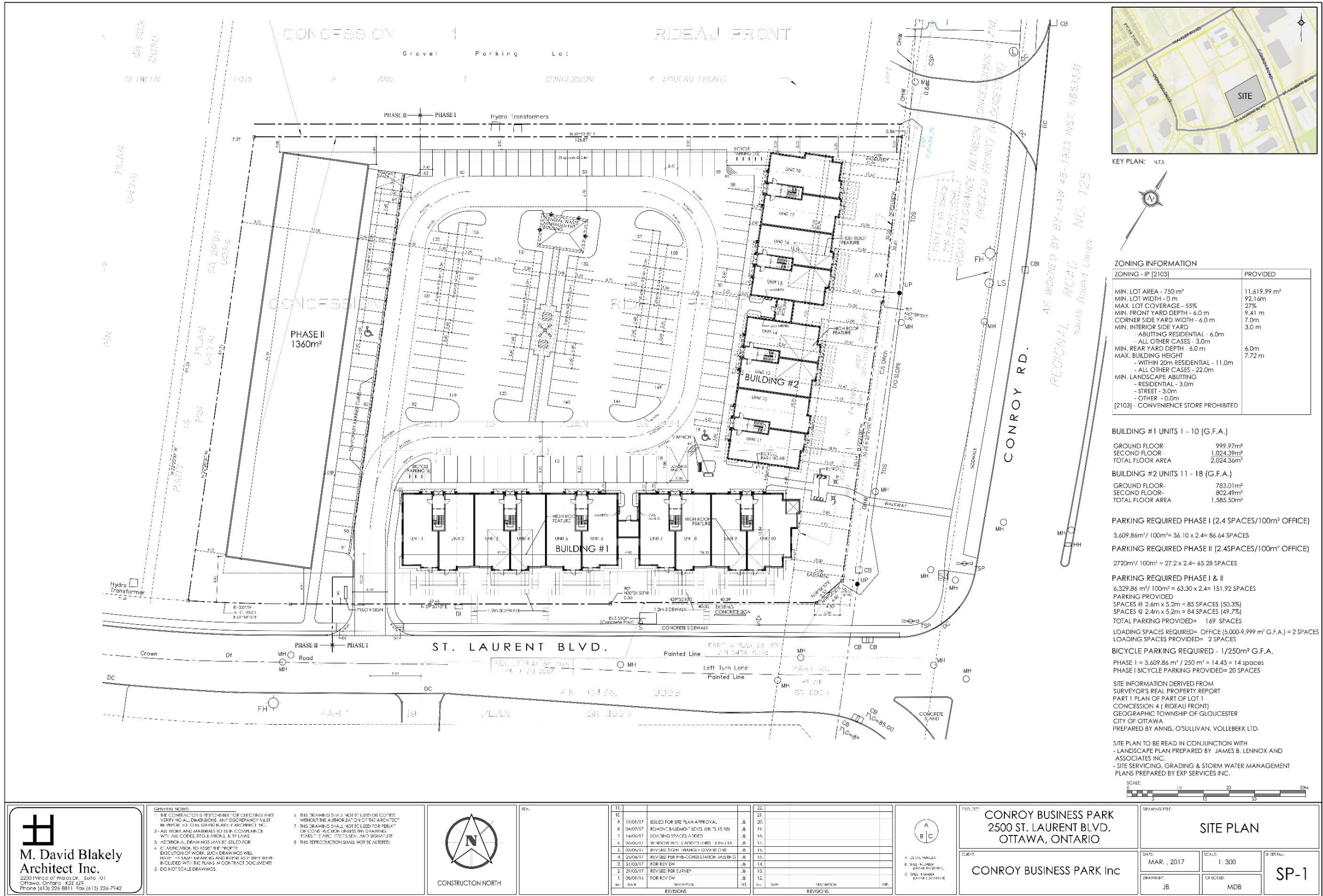
The proposal will provide a pedestrian-oriented environment at the northwest quadrant of the Conroy Road and St. Laurent Boulevard intersection. A wide walkway that connects to the MUP along Conroy Road will provide a key-hole effect entry into the site between the two proposed linear buildings. This pedestrian entrance will open into a courtyard containing active building frontage for units 8-10, as well as bicycle parking, seating areas, and a central landscaped focal point. Two areas for bicycle parking are appropriately located on site and are accessed through internal sidewalks.

Along the south side of the property will be three private sidewalks that provide connection to units 1-6 inclusive, and units 7-10 inclusive. These internal sidewalks will access courtyards along the south side of each individual unit within proposed Building #1. This design provides for an active street frontage across the entirety of the site's Phase 1 area fronting St. Laurent Boulevard. The sidewalk along St. Laurent Boulevard will enter the site on both sides of the proposed vehicle entrance, and will continue along the front of the proposed buildings, providing a continuous connection from the City sidewalk to the entrance of any of the site's office units. The boulevard of

the centre parking strip has been designed to provide a large and well-lit seating area with ample tree cover, and a sidewalk connection to Building #1.

The site's pedestrian friendly environment is complemented by a mix of hard and soft landscaping which serves both an aesthetic and functional purpose (i.e., active street frontages and entrances, thermal moderating, etc.). The proposal has been designed to optimize the retention of existing trees along the site's perimeter, especially along the east and south property lines. The three central parking strips have been capped with landscaped boulevards containing trees that will have shade the parking lot during warmer months and function as a wind break during cooler months.

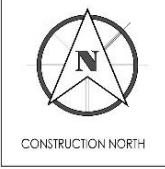
The proposal has been designed with particular care to the needs and desires of both the public and the future tenants of the site and surrounding area.



M. David Blakely
Architect Inc.
2200 Prince of Wales Dr., Suite 101
Ottawa, Ontario K2E 6Z9
Phone (613) 226-8811 Fax (613) 226-7942

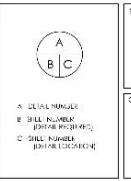
- GENERAL NOTES:**
1. THE CONTRACTOR IS RESPONSIBLE FOR CHECKING AND VERIFYING ALL DIMENSIONS. ANY DISCREPANCY MUST BE REPORTED TO M. DAVID BLAKELY ARCHITECT INC.
 2. ALL WORK AND MATERIALS TO BE IN CONFORMANCE WITH ALL CODES, REGULATIONS, & BY-LAWS.
 3. ADDITIONAL DRAWINGS MAY BE ISSUED FOR CLARIFICATION TO ASSIST THE PROJECT.
 4. EXECUTION OF WORK, SUCH AS DRAWINGS WILL HAVE THE SAME APPEARANCE AND BEHIND AS IF THEY WERE INCLUDED WITH THE PLANS & CONTRACT DOCUMENTS.
 5. DO NOT SCALE DRAWINGS.

6. THIS DRAWING SHALL NOT BE USED OR COPIED WITHOUT THE AUTHORIZATION OF THE ARCHITECT.
7. THIS DRAWING SHALL NOT BE USED FOR PERMITS OR CONSTRUCTION UNLESS THE DRAWING IS SIGNED, SEALED, AND SIGNED.
8. THIS REPRODUCTION SHALL NOT BE ALTERED.



NO.	DATE	DESCRIPTION	BY	CHKD.
11.				
12.				
13.				
14.				
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16.				
17.				
18.				
19.				
20.				
21.				
22.				

NO.	DATE	DESCRIPTION	BY	CHKD.
1.	13/05/17	BOILED FOR SITE PLAN APPROVAL	J.B.	
2.	06/07/17	REVISED BASED ON COMMENTS (REVISED 15/10/17)	J.B.	
3.	14/06/17	LOADING SPACES ADDED	J.B.	
4.	06/06/17	WINDOW W/15 IS ADDED TO UNIT 15	J.B.	
5.	05/06/17	REVISED SIGN (TRIANGLE DIMENSIONS)	J.B.	
6.	25/04/17	REVISED FOR PRE-CONSULTATION MEETING	J.B.	
7.	31/03/17	FOR REVIEW	J.B.	
8.	29/03/17	REVISED FOR SURVEY	J.B.	
9.	05/02/16	FOR REVIEW	J.B.	
10.				



CONROY BUSINESS PARK
2500 ST. LAURENT BLVD.
OTTAWA, ONTARIO

CLIENT:
CONROY BUSINESS PARK Inc

DRAWING TITLE:
SITE PLAN

DATE:
MAR., 2017

SCALE:
1:300

DRAWN BY:
J.B.

CHECKED BY:
M.D.B.

SP-1

Figure 11. Proposed Site Plan



**M. David Blakely
Architect Inc.**
2200 Prince of Wales St., Suite 101
Ottawa, Ontario K2M 6P7
Phone: (613) 226-8811 Fax: (613) 226-7547

GENERAL NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND VARIATIONS. ANY DISCREPANCY MUST BE REPORTED TO THE ARCHITECT IMMEDIATELY.
2. ALL WORK AND MATERIALS TO BE IN COMPLIANCE WITH ALL CODES, ORDINANCES AND REGULATIONS.
3. ADDITIONAL DRAWINGS MAY BE REQUIRED FOR PERMITS AND/OR CONSTRUCTION. THE ARCHITECT WILL BE NOTIFIED OF ANY SUCH REQUIREMENTS.
4. DO NOT SCALE DRAWINGS.
5. THIS REPRESENTATION SHALL NOT BE ALTERED.

NOTES:

1. THIS DRAWING IS A PRELIMINARY DESIGN. IT IS NOT TO BE USED FOR CONSTRUCTION WITHOUT THE ARCHITECT'S SEAL AND SIGNATURE.
2. THE ARCHITECT'S SEAL AND SIGNATURE ARE REQUIRED FOR ALL PERMITS AND FOR CONSTRUCTION.
3. THE ARCHITECT'S SEAL AND SIGNATURE ARE REQUIRED FOR ALL PERMITS AND FOR CONSTRUCTION.

NO.	DATE	DESCRIPTION	BY	DATE	DESCRIPTION	BY
1	15/03/17	FOR REVIEW	JB	15		
2	15/03/17	FOR REVIEW	JB	15		
3	15/03/17	FOR REVIEW	JB	15		
4	15/03/17	FOR REVIEW	JB	15		
5	15/03/17	FOR REVIEW	JB	15		
6	15/03/17	FOR REVIEW	JB	15		
7	15/03/17	FOR REVIEW	JB	15		
8	15/03/17	FOR REVIEW	JB	15		
9	15/03/17	FOR REVIEW	JB	15		
10	15/03/17	FOR REVIEW	JB	15		

PROJECT: CONROY BUSINESS PARK
2500 ST. LAURENT BLVD.
OTTAWA, ONTARIO

CLIENT: CONROY BUSINESS PARK Inc

DESIGNER: M. David Blakely Architect Inc

DATE: MAR. 2017

SCALE: 1:100

BUILDING NO.: A1

Figure 12. Elevations of Building #1



Figure 13. Elevations of Building #2

4. PROVINCIAL POLICY STATEMENT, 2014

The Provincial Policy Statement, 2014 (PPS) provides policy direction on planning matters for the Province of Ontario. Decisions affecting all planning matters shall be consistent with the Provincial Planning Policies. The proposed business park development at 2500 St. Laurent Boulevard is consistent with the policies of the PPS as demonstrated below.

Policies:

Section 1.1.1 of the PPS sets out the criteria whereby healthy, livable, and safe communities are sustained. In short, these are: to promote efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term; avoiding development and land use patterns which may cause environmental or public health and safety concerns; promote cost-effective development patterns; improve accessibility for persons with disabilities and older persons by identifying, preventing, and removing land use barriers which restricts their full participation in society; ensuring there is necessary infrastructure; and, promoting development and land use patterns that conserve biodiversity.

In the above list of criteria, there is a large emphasis on efficient development that is safe, compatible and complementary, respects the natural environment, uses available infrastructure, and provides for the needs of the community both in the short term and long term.

- The proposal provides a safe pedestrian environment that is animated with human-scaled façade treatment and materiality, as well as well-lit and identifiable entrances and walkways.
- The proposal provides for office type uses that are compatible and complementary to the existing uses and built form of the surrounding development within the Ottawa Business Park.
- The proposal includes a range of different office unit sizes to be condominiumized which provides flexibility for meeting existing and future employment needs.
- The proposal respects the natural environment by retaining site trees where possible, and by providing additional vegetation that will serve both an aesthetic and functional purpose.

Section 1.3.2.1 of the PPS states that planning authorities shall plan for, protect and preserve employment areas for current and future uses and ensure that the necessary infrastructure is provided to support current and projected needs.

Section 1.3.2.3 of the PPS states that planning authorities shall protect employment areas in proximity to major goods movement facilities and corridors for employment uses that require those locations.

The proposal is to develop an underutilized and vacant corner lot with employment type land uses. The site is located in the Ottawa Business Park which is in proximity to Highway 417 and a major east-west CN rail corridor. The introduction of office use to the site demonstrates consistency with the policies of the PPS with respect to planning for, protecting, and preserving employment areas within proximity to major goods movement facilities and corridors.

Section 1.5.1 of the PPS states that healthy, active communities should be promoted by: planning public streets, spaces and facilities to be safe, meet the needs of pedestrians, foster social interaction and facilitate active transportation and community connectivity.

The site is located within a business park setting that is characterized by large lots, large setbacks from the street, ample surface parking, and wide road allowances. These types of settings are typically auto-oriented. To meet the needs of the users of this area, the proposal includes ample vehicle parking which is easily accessed from the site's internal ring road, and is complemented both aesthetically and functionally by the trees and landscaping proposed for the site.

The site has also been designed with a major focus on pedestrian connectivity, especially along the site's southeast corner at the intersection of Conroy Road and St. Laurent Boulevard where a multi-use pathway, sidewalks, and signalized cross-walks provide connection between the site and nearby public transit stops. There will be six separate walkways entering the site from City-owned walkways along Conroy Road and St. Laurent Boulevard. These walkways, along with the orientation of the buildings and the internal setting of the parking, help to animate the street frontage at a human scale, a valuable feature which is not typically found elsewhere throughout the surrounding business park.

Section 1.6.6 of the PPS states that planning for sewage and water services shall direct and accommodate expected growth or development in a manner that promotes the efficient use and optimization of existing: municipal sewage services and municipal water services.

Section 1.7.1 of the PPS states that long-term economic prosperity should be supported by: promoting opportunities for economic development and community investment-readiness [...].

The site is located within the City's urban policy area, and has access to full municipal services. As an underutilized corner lot, the site is a desirable location for development within the surrounding business park setting that has existed for over 30 years. The proposal will introduce a business park of high quality design that will have flexibility in its unit sizes, and will help to influence the development of the vacant lands abutting it to the north and west.

The proposal is consistent with the applicable policies of the PPS, 2014.

5. CITY OF OTTAWA OFFICIAL PLAN

Schedule B of the City of Ottawa Official Plan designates the site at 2500 St. Laurent Boulevard as General Urban Area, as shown in Figure 14. The general character of this designation is described in Section 3.6.1 of the Official Plan, and summarized as such:

“The General Urban Area designation permits the development of a full range and choice of housing types to meet the needs of all ages, incomes and life circumstances, in combination with conveniently located employment, retail, service, cultural, leisure, entertainment and institutional uses. This will facilitate the development of complete and sustainable communities. A broad scale of uses is found within this designation, from ground-oriented single-purpose to multi-storey mixed-use; from corner store to shopping centre.”

The proposed business park for the site represents an employment use which conforms with the permitted uses of the Official Plan’s General Urban Area designation.

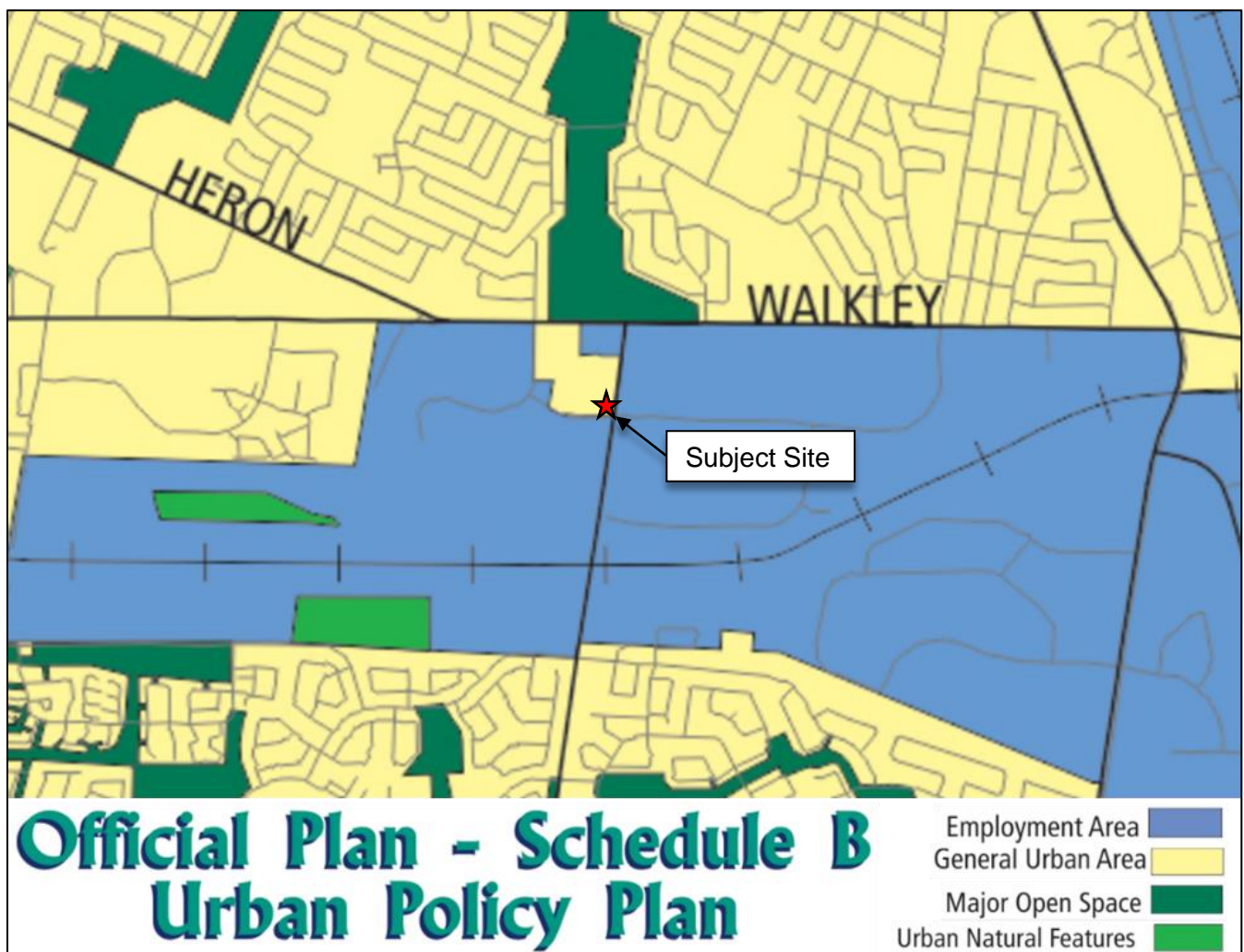


Figure 14. Official Plan – Schedule B Urban Policy Plan (General Urban Area)

5.1 Section 3.6.1 – General Urban Area

Policies

1. General Urban Area areas are designated on Schedule B. The General Urban Area designation permits all types and densities of housing, as well as employment, retail uses, service, industrial, cultural, leisure, greenspace, entertainment and institutional uses.

The proposal consists of office use buildings representing an appropriate employment use for the land.

2. The evaluation of development applications, studies, other plans and public works undertaken by the City in the General Urban Area will be in accordance with Section 2.5.1 and Section 4.11.

The proposal conforms to the Policies of Section 2.5.1 and Section 4.11 as demonstrated in subsection 5.2 and 5.3 of this report.

The proposed development conforms to the policies of Section 3.6.1 of the Official Plan.

5.2 Section 2.2.2 – Managing Growth Within the Urban Area

Section 2.2.2 of the Official Plan deals with the City's strategic directions for managing future growth within the City's Urban Area, which includes the subject site.

Policy 14 of this Section speaks to intensification outside of target areas, and states the following:

14. The City also supports intensification throughout the urban area, including areas designated General Urban Area. The City will promote opportunities for intensification in the following cases, provided that all other policies in the Plan are met:

- d. Lands currently or formerly used as parking lots or other extensive storage purposes.

The proposed development represents a candidate location for employment intensification within the City's urban area but outside of its target areas. The proposed development conforms to the policies of Section 2.2.2 of the Official Plan.

5.3 Section 2.5.1 – Urban Design And Compatibility

Section 2.5.1 of the Official Plan deals with urban design and compatibility. This Section contains a number of design objectives and principles to be addressed through the built form and functionality of a site. These design objectives include:

- To enhance a sense of community by creating and maintaining places with their own distinct identity.
- To define quality public and private spaces through development.
- To create places that are safe, accessible and are easy to get to, and move through.
- To ensure that new development respects the character of existing areas.

- To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.
- To understand and respect natural processes and features in development design.
- To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.

The proposed business park development addresses the above design objectives, as described below:

- Reflects a thorough and sensitive understanding of place, context and setting by providing a compatible and complementary use and site design that fits well and works well with the existing character, patterns and massing of the immediate and surrounding area.
- Clearly defines, connects and enhances public and private space by way of a pedestrian oriented built form complemented by high-quality building design, site design, and landscaping.
- Reduces the visual impact of parking by locating it internally and having active street frontages that provide multiple access points for pedestrians, including an attractive pathway and vista at the corner of Conroy Road and St. Laurent Boulevard.
- Provides a safe, comfortable, and functional environment that is well-lit, open, and highly accessible for both pedestrians and vehicles of varying needs (i.e., internal parking, ring road design to increase flow efficiency, separated access for pedestrians, high visual exposure for bicycle parking, etc.).
- Retains a number of existing site tree's, proposes new trees, and complements the natural environment by providing landscaping that serves both a functional and aesthetic purpose (i.e., framing pathways and public spaces, shading parking areas, providing infiltration for stormwater, etc.).

The proposed development conforms to the policies of Section 2.5.1 of the Official Plan.

5.4 Section 4.11 – Urban Design And Compatibility

Section 4.11 of the Official Plan regarding urban design and compatibility generally deals with the criteria of height, massing, scale relationship, and building/lot relationships within the context of built form and operational characteristics, such as traffic, access, and parking.

The City will evaluate the compatibility of development applications on the basis of the following compatibility criteria.

a. Traffic: Roads should adequately serve the development, with sufficient capacity to accommodate the anticipated traffic generated. Generally, development that has the potential to generate significant amounts of vehicular traffic should be located on arterial or major collector roadways so as to minimize the potential for traffic infiltration on minor collector roadways and local streets;

The site is a corner lot at the signalized intersection of St. Laurent Boulevard (urban collector) and Conroy Road (urban arterial). The proposal is for a business park development, which is expected to draw the majority of its traffic from employees and patrons. Conroy Road is a multi-lane arterial road with north and southbound left turn lanes at the intersection with St. Laurent Boulevard. The portion of St. Laurent Boulevard serving the site is used primarily by employees and patrons of the surrounding

business park establishments, and in conjunction with Conroy Road, is expected to adequately serve the proposed development.

b. Vehicular Access: The location and orientation of vehicle access and egress should address matters such as the impact of noise, headlight glare and loss of privacy on development adjacent or immediately opposite. Vehicular access and egress for development that has the potential to generate a significant amount of vehicular traffic should be oriented on streets other than local streets, wherever the opportunity exists, considering traffic safety and other transportation objectives of this Plan; [Amendment #76, OMB File #PL100206, August 18, 2011]

The proposal does not present any impacts relating to noise, light glare and loss of privacy with respect to adjacent properties. The only vehicle access point to the site is proposed along St. Laurent Boulevard, and is to lead to an internal parking lot that is buffered from the adjacent non-sensitive properties and roadways by the orientation of the proposed buildings and existing and proposed landscaping.

c. Parking Requirements: The development should have adequate on-site parking to minimize the potential for spillover parking on adjacent areas. A range of parking forms, including surface, decked, and underground, should be considered taking in account the area context and character. Opportunities to reduce parking requirements and promote increased usage of walking, cycling and transit will be pursued, where appropriate, particularly in the vicinity of transit stations or major transit stops in accordance with the provisions of Section 4.3; [Amendment #76, OMB File #PL100206, August 18, 2011]

The proposed development will provide 169 surface parking spaces (plus 2 accessible spaces); whereas, 152 spaces are required (plus 2 accessible spaces). The site is located in a business park setting which relies heavily on personal vehicles for transportation. The 169 proposed parking spaces meets the expected demand for vehicle parking, while providing an environment that encourages public and active modes of transit. The proposal encourages alternative modes of transit by providing bicycle parking and pedestrian pathways in visible, accessible and well-lit locations. Although Phase 1 of the proposal provides only 20 of the 25 total required bicycle parking spaces, it is expected that Phase 2 will include 5 or more additional spaces to meet or exceed the minimum requirement.

e. Loading Areas, Service Areas, and Outdoor Storage: The operational characteristics and visual appearance of loading facilities, service areas (including garbage), parking, and areas for the outdoor storage of goods or materials should be mitigated using a variety of methods (e.g., location, containment, screening, berms, and/or landscaping). These uses and activities should be located away from residences where possible.

The site has been designed with a large emphasis on operational characteristics and aesthetics. Proposed parking for the site is to be located around and within an internal ring-road to optimize site functionality and to have active street frontages that emphasize the pedestrian environment. Garbage is to be stored within a confined building located in a central and easily accessible area of the site's internal parking, and screened by vegetation. Service and loading areas are appropriately located at

the terminus of parking strips to allow for optimal access to the pedestrian grade environment which connects directly to the office fronts.

f. Supporting Neighbourhood Services: The development should contribute to or be adequately served by existing or proposed services and amenities such as health facilities, schools, parks and leisure areas. Where the proposed development itself is to contribute such services and amenities, they should be of a scale appropriate to the needs and character of the area. [Amendment #28, July 13, 2005] [OMB decision #2649, September 21, 2006]

The proposed development will contribute to existing and proposed services and amenities at a scale that is appropriate for the needs and character of the area. As a proposed business park consisting of varying office type uses, the proposal will fit well and work well in the area bordered by mixed use development along Walkley Road to the north and light industrial and business park type uses along Conroy Road and St. Laurent Boulevard to the east, south and west.

The proposed development conforms to the policies of Section 4.11 of the Official Plan.

5.5 Official Plan Amendment 150

On December 11, 2013, City Council adopted Official Plan Amendment 150 (OPA 150) to implement the completion of the five-year review of the Official Plan. While not technically in force, OPA 150 provides Council's intended and adopted policy direction.

With respect to the proposal, there are no material differences in the applicable policies of the currently enforced Official Plan and the policies of OPA 150. The proposal demonstrates conformity with the applicable policies of OPA 150 that relate to Section 3.6.1 – General Urban Area, 2.5.1 – Urban Design and Compatibility Designing Ottawa, and 4.11 – Urban Design and Compatibility Compatible Development.

The proposed development achieves the intent and purpose, and conforms to the policies of the City of Ottawa Official Plan and OPA 150.

6. ZONING BY-LAW

In the City of Ottawa Zoning By-law 2008-250 the subject site is zoned IP[2103] – Business Park Industrial Zone. The proposed business park to consist of office type uses is permitted within the IP[2103] zone, and as described below, represents an intended and desirable use of the land.



Figure 15. Zoning map highlighting the site within the IP[2103] Zone

The purpose of the IP Zone is to:

- (1) Accommodate mixed office, office-type uses and low impact, light industrial uses in a business park setting, in accordance with the Enterprise Area designations of the Official Plan or, the Employment Area or the General Urban Area designation where applicable.

Table 2 displays the zone provisions for the IP Zone, all of which are complied with by the proposal.

Table 2. IP Zone Provisions

Provisions	Required/Permitted	Proposed
(a) Minimum lot area	750 m ²	11,619.99 m ²
(b) Minimum lot width	No Minimum	92.16 m
(c) Maximum lot coverage setbacks	55%	27%
(d) Minimum front yard and corner side yard	6 m	9.41 m & 7 m
(e) Minimum interior side yard	3 m	3 m
(f) Minimum rear yard	6 m	6 m
(g) Maximum floor space index	2	≤ 2
(h) Maximum building height	22 m	7.72 m
(i) Minimum width of landscaping	(ii) Abutting a street = 3 m (iii) In all other cases = No minimum	≥ 3 m ≥ 0 m

With respect to parking, Table 4 demonstrates how the proposal complies with the City's Zoning By-law and the Traffic and parking By-law. The final plan for Phase 2 is to include at least seven additional bicycle parking spaces.

Table 3. Parking Requirements

Parking for Office Use	PARKING RATE	REQUIRED			PROVIDED		
		Phase 1	Phase 2	TOTAL	Phase 1	Phase 2	TOTAL
VEHICLE	2.4 spaces per 100 m ² of GFA	86.64	65.28	152	141	28	169
LOADING	1 space for a GFA between 350 m ² – 4,999 m ²	1	1	2	1	1	2
ACCESSIBLE / DISABLED	1 space for 20-99 provided vehicle spaces	1	1	2	1	1	2
BICYCLE	1 space per 250 m ² of GFA	14.43	10.88	25	20	7	≥ 25
Notes <ul style="list-style-type: none"> • Phase 1 = 3,609.86 m² • Phase 2 = 2,720 m² • Total Buildout = 6,329.86 m² 							

The proposed development achieves the intent and purpose, and complies with the applicable provisions of the City of Ottawa Zoning By-law 2008-250.

7. SUPPORTING STUDIES

7.1 Plan of Condominium

Following Site Plan Control Approval, the intent is to submit an application for a Phased Plan of Condominium (the 'Condo'). The Condo will be based upon the development phasing which consists of the two phases outlined in Section 3 of this report.

The Condo is subject to the same regulations as a Plan of Subdivision with respect to Section 51 of the Ontario Planning Act. The rationale for the Condo is the same as discussed for the Site Plan Control application within this report.

With respect to ownership structure, the Condo will allow for additional tenure options for each individual unit of the development and Condo (i.e., freehold ownership and/or leasing). The layout and extent of the Condo units will be influenced by market demands, and will be determined following Site Plan Control Approval.

7.2 Site Servicing & Storm Water Management Report

A Stormwater Management and Servicing Report was prepared by exp. Services Inc., dated September 19, 2017. The following is a summary of the report's findings:

- A 150mm diameter water service connected to the 300mm municipal water main on St. Laurent Boulevard will adequately service the proposed development.
- An existing Fire Hydrant on St. Laurent Boulevard and Conroy Road will provide fire protection for the proposed development.
- A 150mm diameter sanitary service connected to the 375mm diameter municipal sanitary sewer on St. Laurent Boulevard will adequately service the proposed development.
- SWM for the proposed development will be achieved by restricting all storms up to the 100-year post development flow to the pre-development 5-year release rate of 111.15L/s.
- Required on-site SWM storage volumes will be achieved using the surface storage in the parking-lots and underground pipes/structures.
- Quality control will be provided by the existing McEwen Creek Stormwater Management Facility which received flows from the 1500mm storm sewer on St. Laurent Boulevard. No additional water quality measures are proposed.
- Temporary erosion and sediment control measures for the subject site have been identified.
- Overland flow routes have been provided for the subject site.
- During all construction activities, erosion and sedimentation shall be controlled.

Please see the report for further details.

7.3 Transportation Impact Brief

A Transportation Brief was prepared by Stantec Consulting Ltd. dated September 6, 2017. The report included a review and forecast of the traffic conditions surrounding the site, and contained recommendations relating to the current proposal.

The Brief indicated the following conclusion:

- There are no foreseeable transportation issues related to the site access location or general site circulation.
- The study area intersections currently operate acceptably under 2-17 existing conditions.
- The proposed office development is expected to generate approximately 72 vehicle trips during the AM peak hour and 99 vehicle trips during the PM peak hour.
- With the addition of the anticipated site traffic generated by the proposed development, the study area intersections are expected to operate acceptably under 2021 total future conditions.
- The study area intersections are expected to operate acceptably under 2026 ultimate conditions.

Based on the transportation evaluation and the negligible impacts that have been identified in this Transportation Brief, the proposed office development at 2500 St. Laurent Boulevard is not anticipated to have a significant impact on the transportation network and should be permitted to proceed.

Please see the report for further details.

7.4 Phase One ESA

A Phase One Environmental Site Assessment (ESA) was prepared by exp. Services Inc., dated May 10, 2017. The purpose of the ESA was to research the past and current use of the subject site and adjacent properties and identify any environmental concerns with the potential to have impacted the subject site.

The ESA determined that the site has historically remained vacant and currently contains remnant asphalt and gravel (from the previous use as a parking lot) as well as vegetative growth. Based on results of the Phase One ESA at 2500 St. Laurent Boulevard no areas of potential environmental concern (APEC) were identified. The report recommended that no further environmental investigation would be required.

Please see the report for further details.

7.5 Geotechnical Investigation

A Geotechnical Investigation was prepared by exp. Services Inc., dated July 5, 2017. The report included a review of the geotechnical conditions and contained the following considerations and conclusions:

- Based on a review of the engineering properties of the silty clay, it is considered that the proposed grade raise at the site of 0.2 to 0.8 m is acceptable in conjunction with the footings designed as noted below and in the attached report.
- The investigation revealed that the geotechnical conditions at the site are suitable for the construction of the two-storey structures without basements.
- The floor slabs for the basementless buildings may be constructed as slabs-on-grade.

- Excavations at the site, above the groundwater table, may be undertaken as open cut provided that the excavation sides are sloped back at 1H:1V from the bottom of the excavation.
- The on-site soils to be excavated are anticipated to consist of gravelly sand fill, brown silty clay (desiccated crust) and grey silty clay.
- Pavement structure thickness for heavy duty traffic parking lots and access roads may consist of 90 mm of asphaltic concrete underlain by 150 mm of Granular A base and 600 mm of Granular B Type II sub-base.

Please see the report for further details.

7.6 Transportation Noise Study

A Traffic Noise Assessment was prepared by Gradient Wind Engineering Inc. dated August 14, 2017. The purpose of the assessment was to calculate the future noise levels provided by local traffic and experienced by the study building to ensure that interior noise levels do not exceed the allowable limits specified by the City of Ottawa's Environmental Noise Control Guidelines (ENCG).

The report concluded that the development will require central air condition or a similar mechanical system. Installation of central air condition will allow occupants to keep windows and exterior doors closed while maintaining a comfortable indoor environment. In addition to ventilation requirements, Warning Clauses will be required in all Lease, Purchase and Sale Agreements, as detailed in the report.

Upgraded building components are required where noise levels exceed 65 dBA (see report for recommendation). Only the daytime range of traffic noise levels indicates an exceedance of 65 dBA (daytime range of 55 to 67 dBA).

Please see the report for further details.

8. CONCLUSION

The site of the proposed Conroy Business Park Inc. is located in the City's East Industrial Area. This area is characterized by large lots containing light industrial and business park type uses, as well as ample parking and large setbacks from abutting road allowances.

Specifically, the site is situated at the corner of St. Laurent Boulevard and Conroy Road. Since the early 2000s, the site has remained an underutilized greyfield, that was historically use for parking and crop cultivation. The site is proposed to be developed in a phased approach that will result in a desirable land use that is in keeping with the planned function of the site and surrounding area.

The proposed development will provide three multi-unit office buildings that will account for a cumulative gross floor area of 6,329.86 m². Building #1 and #2 will be developed as part of Phase 1 of the proposal, and will account for a total of 18 units to be condominiumized (Units 1-10 in Building #1 and Units 11-18 in Building #2). Building #3 will be developed as part of Phase 2, and is expected to be designed to similar specifications as Buildings #1 and #2.

The proposed development is consistent with the Provincial Policy Statement 2014, conforms to the City of Ottawa Official Plan, complies with the City of Ottawa Zoning By-law, and represents a land use and site design that will fit well and work well within the context of the immediate and surrounding area.

The proposal represents good land use planning through a compatible, complementary and flexible use of land, and is supported from a land use planning perspective.

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