

Planning Rationale Report

Urbandale Construction
Application for Site Plan Control
800 Ralph Hennessy Avenue,
Southwest corner of
Earl Armstrong Road and
Ralph Hennessy Avenue



Prepared for:
Urbandale Construction

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September 8, 2017

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1.0 INTRODUCTION

This Report has been prepared in support of an application for Site Plan Control approval on behalf of Urbandale Construction for property located at 800 Ralph Hennessy Avenue in Riverside South Community.

The purpose of this application is to permit 8 four storey buildings with a total of 68 stacked townhouse units to be built on this site.

On February 22, 2017, Stantec Consulting Ltd. and Urbandale Construction staff met with City of Ottawa Planning, Infrastructure and Economic Development Department staff to discuss the proposed site plan submission for the site. Over the past months, Stantec and Urbandale have worked with various consultants to prepare the required plans and documents. The site plan incorporates information from the required studies.

2.0 SITE LOCATION, SITE CONTEXT AND DEVELOPMENT PROPOSAL

2.1 SITE LOCATION

The site is located at the South-West corner of Earl Armstrong Road and Ralph Hennessy Avenue as shown on Figure 1 to this Report. The site is rectangular, having a width of 76 meters abutting Ralph Hennessy Avenue, a depth of 137 meters along Earl Armstrong Road and an area of 1.1 hectares (10959 sq. m).

The site fronts onto Ralph Hennessy Avenue, a 26m wide collector road. Earl Armstrong Road is a four lane divided arterial road.

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Figure 1 – Location Map

2.2 SITE CONTEXT

To the immediate east of the site, across Ralph Hennessy Avenue is vacant land at 801 Ralph Hennessy Avenue (Block 221, Plan 4M-1573) owned by Urbandale Construction. This site is in the same Residential Fourth Density R4Z Zone as the subject site and allows stacked townhouses to be built on the site as well.

To the south is Block 201, Plan 4M-1573 that is also in the same zone. This block fronts onto a new local street, Carnelian Crescent that is presently being developed for freehold street townhouses.

To the west is a City Fire Station at 910 Earl Armstrong Road. The station is sited on the west half of a 2-hectare property. The eastern half of the property, which directly abuts this site, is Rideau View Park with a treed area that abuts this proposed development.

To the north, across Earl Armstrong Road, are three bungalow semi-detached homes (6 units) fronting onto Bittersweet Place, a cul-de-sac. To the west of Bittersweet Place are additional bungalow semi-detached homes fronting onto Hot Springs Way, another local street. A

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continuous sound attenuation wall exists along the rear lot lines of these homes where they abut Earl Armstrong Road.

2.3 DEVELOPMENT PROPOSAL

The development proposal is a four storey, 68 unit stacked townhouse project. A total of eight buildings are proposed comprised of 6, 8 and 10 units per building. All units have their access by way of a common private driveway onto Ralph Hennessy Avenue. There are four buildings on each side of the driveway. For sound attenuation purposes, the side walls of the end units abut Earl Armstrong Road. Five units in single buildings will face onto Ralph Hennessy Avenue. The remainder of the site frontage on Ralph Hennessy Avenue will be occupied by a stormwater management pond – a dry pond. The site plan provides 136 parking spaces – one in a garage and one in a driveway for each of the 68 units. There are also 15 visitor parking spaces provided. Although the Zoning By-law does not require parking for bicycles on-site since each unit has a garage, there are 68 spaces available for parking – one within each unit's garage. The site development also includes an accessory free-standing garbage room in the interior of the site.

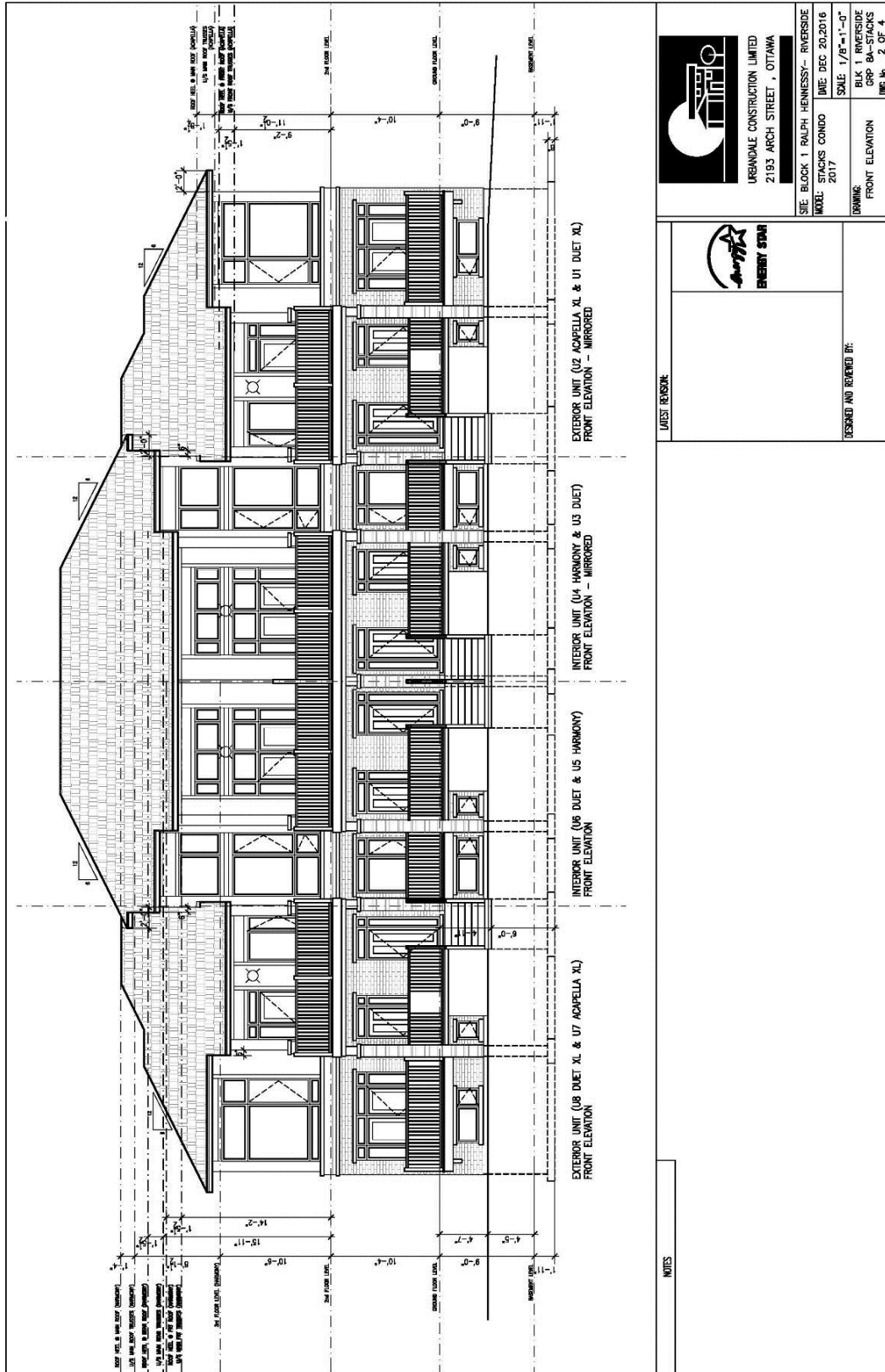
The site plan is designed to comply with the development standards of the Residential Fourth Density R4Z Zone. The R4Z Zone is a zoning category that permits stacked townhouse units and implements the Official Plan and approved Riverside South Community Design Plan

Figure 2 is the Site Plan and Elevation Drawings that show the proposed site development and elevation drawings for all buildings.

Urban services in the form of sanitary sewers and municipal water supply abut the site. Stormwater management measures include an on-site stormwater management pond which will be dry most of the time.

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URBANDALE CONSTRUCTION LIMITED
2193 ARCH STREET, OTTAWA

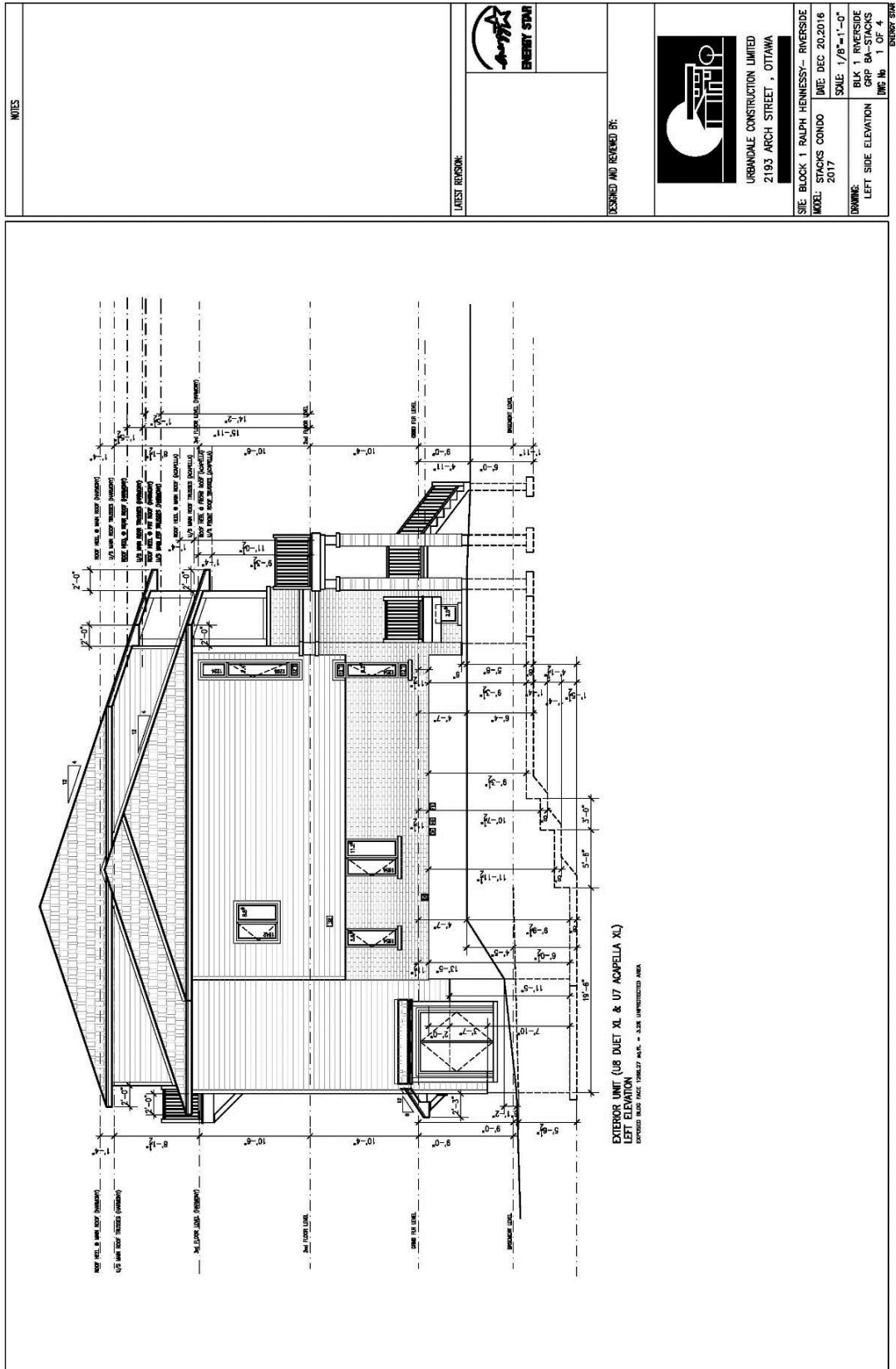
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DESIGNED AND REVIEWED BY:

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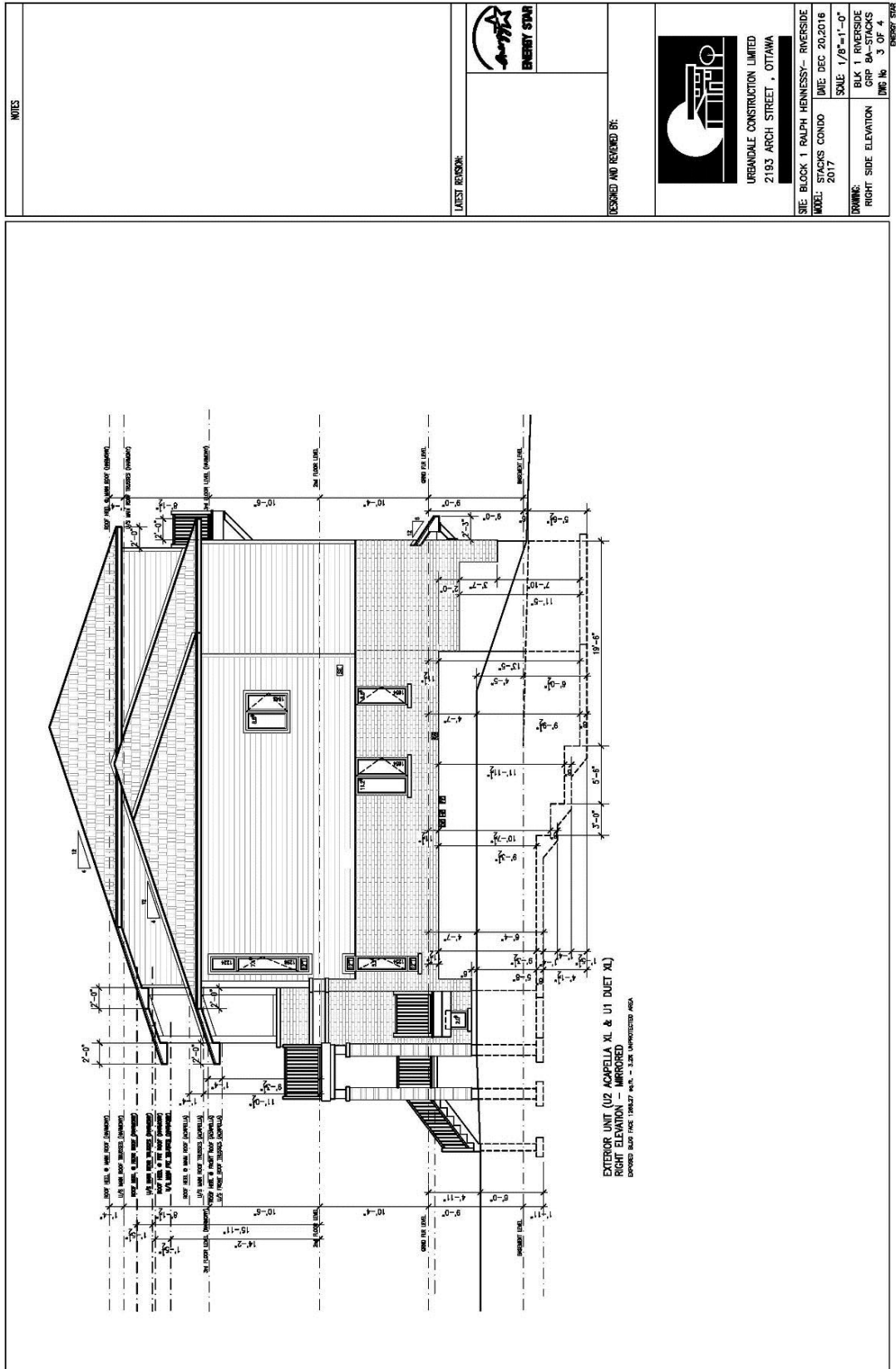
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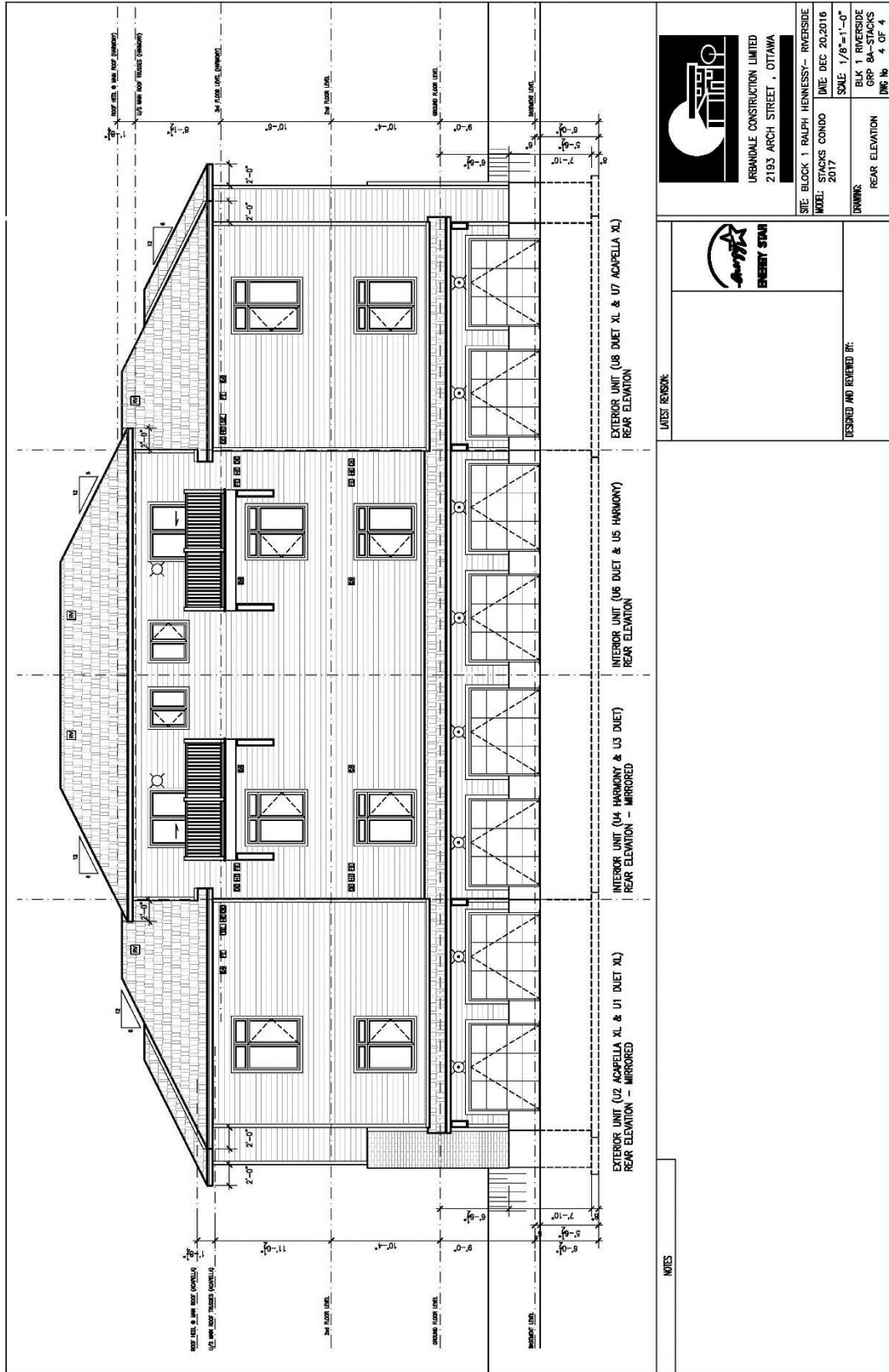
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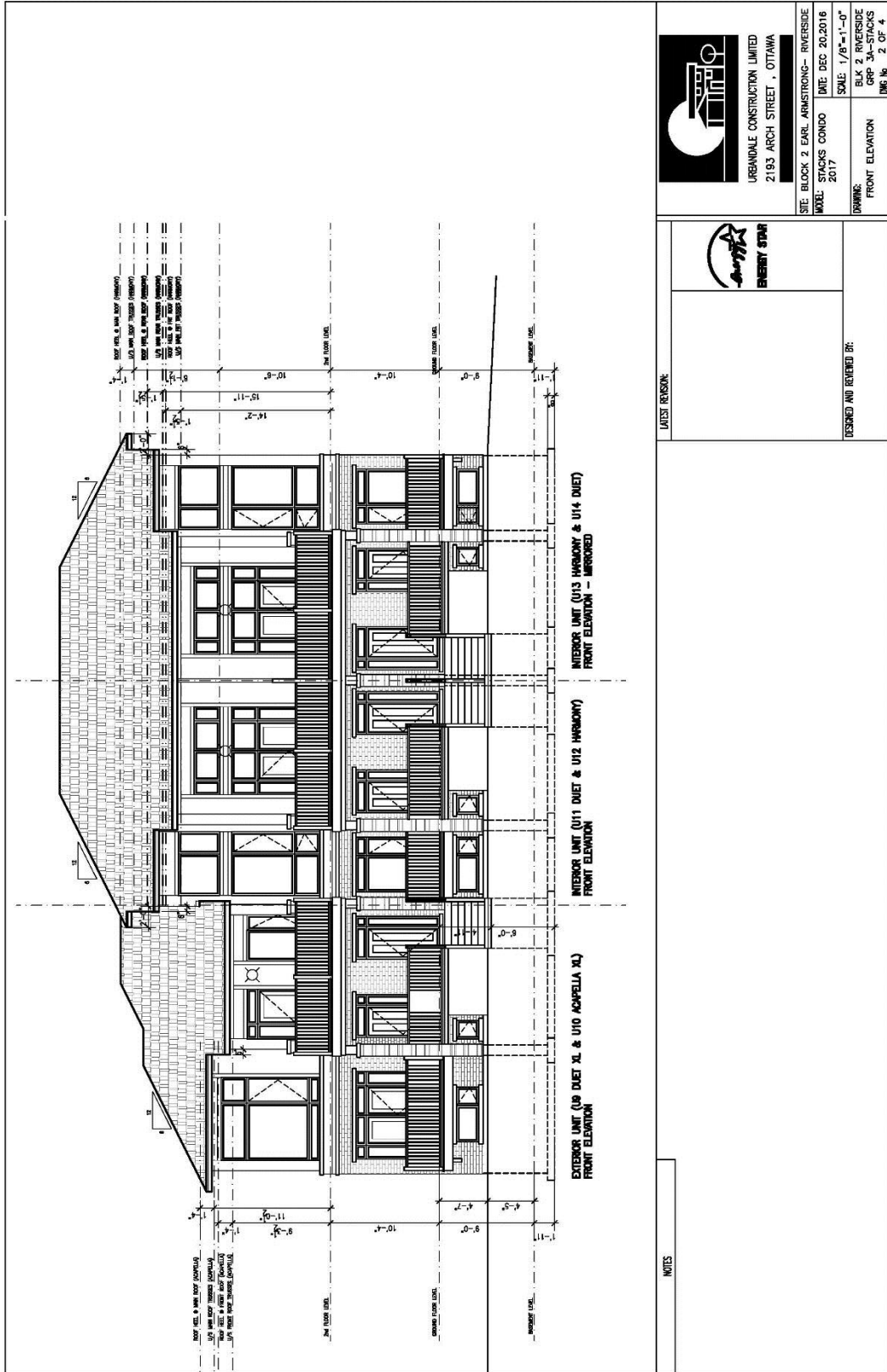
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ENERGY STAR

Urbandale Construction Limited
2193 ARCH STREET, OTTAWA

SITE: BLOCK 2 EARL ARMSTRONG- RIVERSIDE	DATE: DEC 20, 2016	SCALE: 1/8"=1'-0"
MODEL: STACKS CONDO 2017		BLK 2 RIVERSIDE
DRAWING: FRONT ELEVATION		GRP 3A-STACKS
		DWG No. 2 OF 4

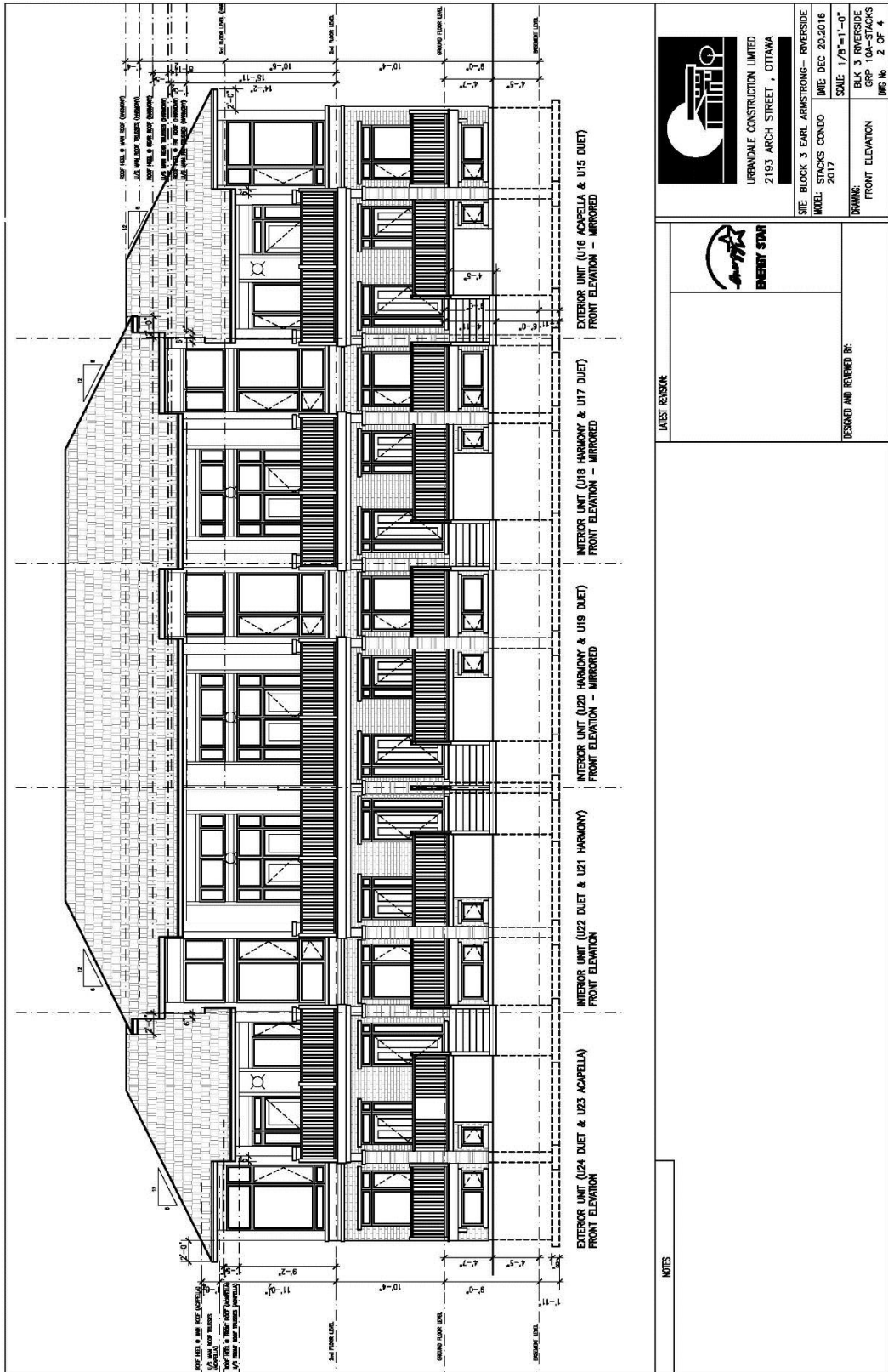
LATEST REVISION:

DESIGNED AND REVIEWED BY:

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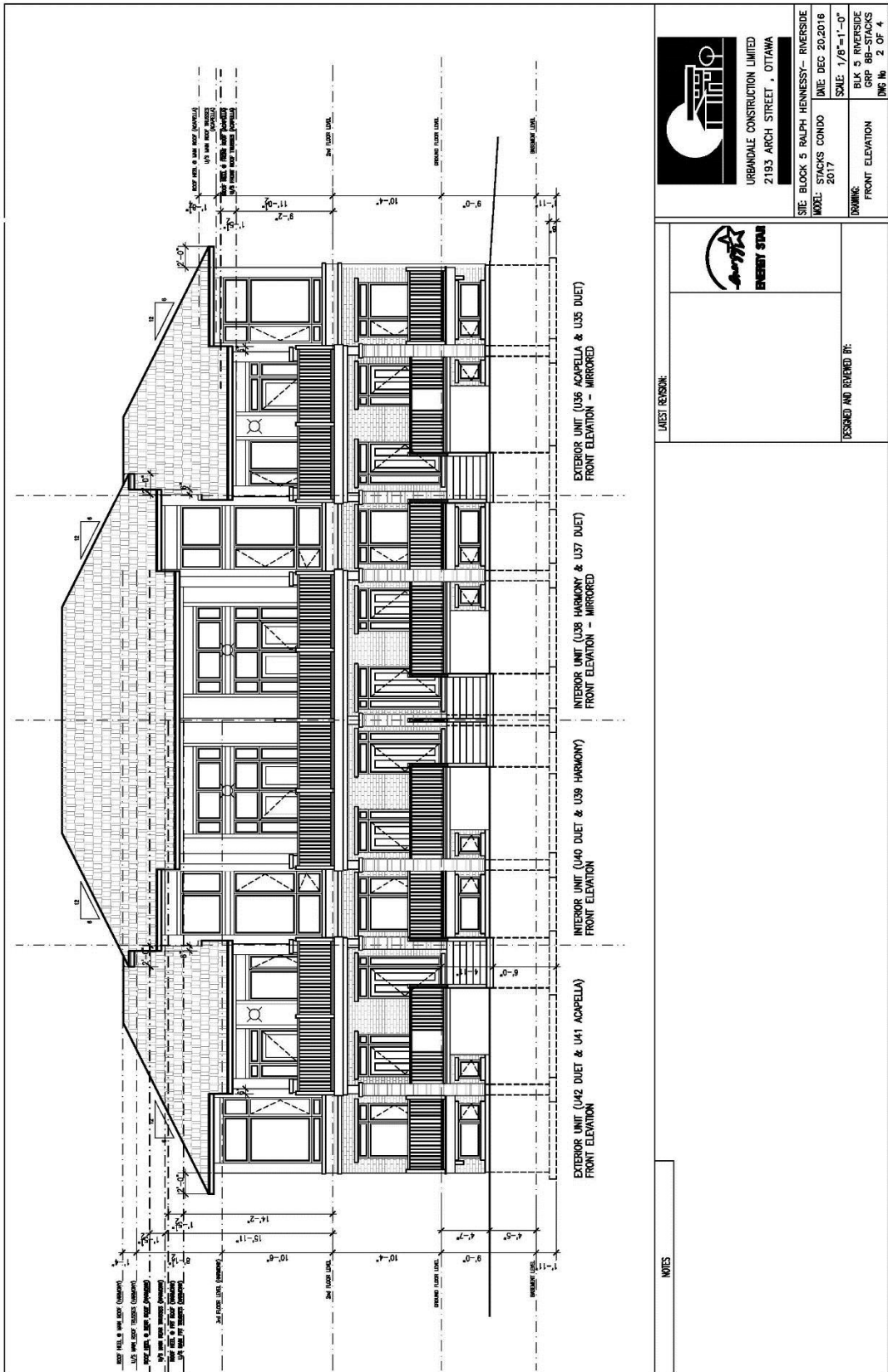
Urbandale Construction, Block 211, Plan 4M-1573
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	<p>URBANDALE CONSTRUCTION LIMITED 2193 ARCH STREET, OTTAWA</p>
<p>DATE: DEC 20, 2016 MODEL: STACKS CONDO 2017</p>	<p>SCALE: 1/8"=1'-0" BLK 4, RIVERSIDE GRP. RESIDUALS DWS NO. 2 OF 4 ENERGY STAR</p>
<p>NOTES</p>	<p>DESIGNED AND REVIEWED BY:</p>

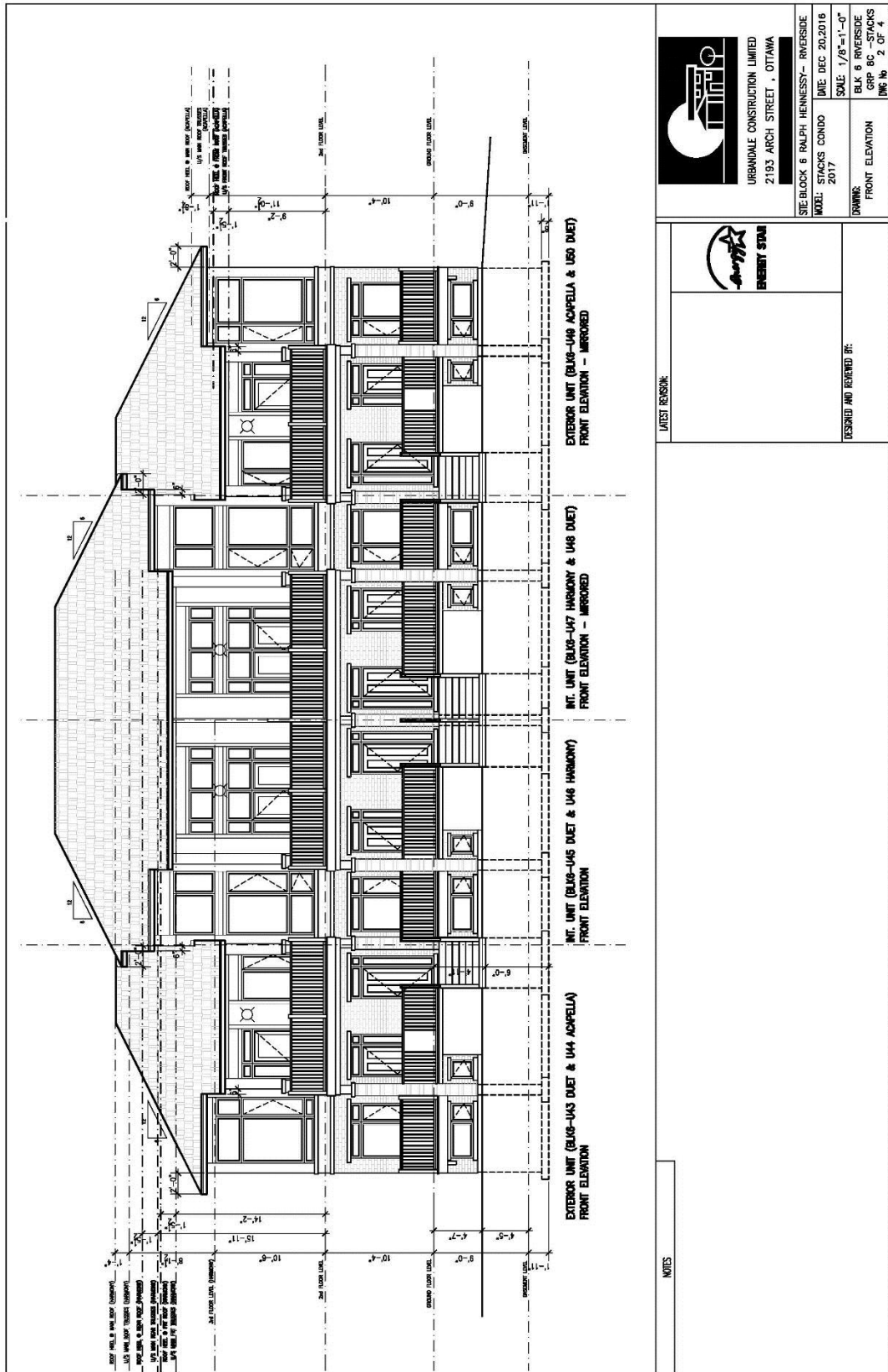
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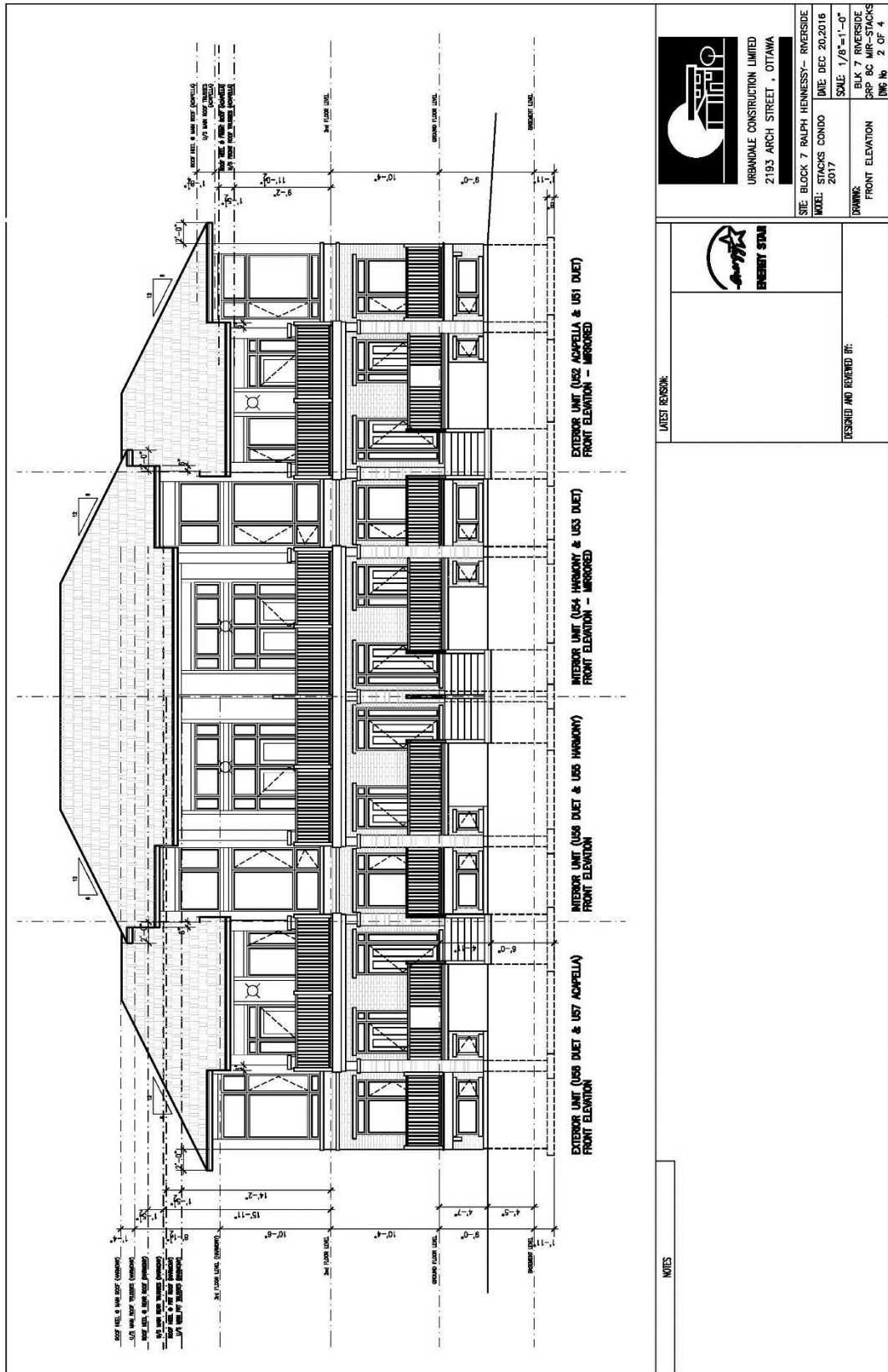
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3.0 POLICY AND REGULATORY FRAMEWORK

3.1 PROVINCIAL POLICY STATEMENT 2014

The Provincial Policy Statement 2014 (PPS) under Section 3 of the *Planning Act* sets out key Provincial interests regarding land use planning. Decisions made by all approval authorities in the Province of Ontario must be consistent with the policies of the Statement.

Section 1.0 Building Strong Healthy Communities

Policy 1.1.1.b) indicates that healthy, livable and safe communities are sustained by accommodating an appropriate range and mix of residential (including second units, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs.

Policy 1.1.1.c) calls on municipalities to avoid development and land use patterns that may cause environmental concerns.

Policy 1.1.1.e) promotes cost-effective development patterns to minimize land consumption and servicing costs.

Policy 1.1.3.1 advises that settlement areas shall be the focus of growth and development, and their vitality and regeneration shall be promoted.

Policies 1.1.3.2 and 1.1.3.3 support intensification where it can be accommodated with sufficient infrastructure. Municipalities are called upon to identify appropriate locations for intensification.

Section 1.6.6.1 has policies for servicing in new developments requiring that they be sustainable, financially viable and protect human health and the environment.

Section 1.6.4.2 indicates that municipal sewage and water services are the preferred for of servicing in settlement areas.

The proposed density of land use in a stacked townhouse project represents a cost-effective land use pattern that minimizes land consumption and servicing costs. There are no on-site features that would cause environmental concerns.

The urban area of the City of Ottawa is a settlement area as defined by the PPS and this site is within the urban area. This site is in an area of the City in which growth and development is promoted.

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In general, the City has implemented the PPS by identifying in the Official Plan areas that are a priority for intensification and by encouraging higher densities of development in greenfield subdivisions such as this site.

Adequate infrastructure supports this site. Piped municipal water and sanitary sewer services are the preferred method of development under the PPS and that is what is proposed for this site.

Section 2.0 Wise Use and Management of Resources

This site is vacant and there are no natural resources on the property that require management.

Section 3.0 Protecting Public Health and Safety

There are no natural hazards or human-made hazards on or near to this site.

In conclusion, approval of this application for Site Plan Control is consistent with and supported by the policies of the PPS 2014.

3.2 CITY OF OTTAWA OFFICIAL PLAN AND DESIGN BRIEF

The City's Official Plan implements the Provincial Policy Statement. Schedule B, Urban Policy Plan designates the site, General Urban Area. Stacked townhouses are a permitted use. The maximum building height under the policies of Section 3.6.1 is four storeys which is proposed in this project.

Figure 3, on the following page, is an excerpt of Schedule B that shows the location of the site.

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Figure 3 – City of Ottawa Official Plan, Schedule B – Urban Policy Plan

3.2.1 DESIGN BRIEF

Section 2.5 of the Official Plan places emphasis on the City building livable communities. Section 2.5.1 provides guidance on matters that will mitigate differences between existing and proposed development. Through good design, compatible development can occur even if the adjacent developments are not the same. They can fit well within their surroundings.

Section 2.5.1 contains a series of Design Objectives that are addressed here:

1. To enhance the sense of community by creating and maintaining places with their own distinct identity.

As part of the Riverside South Community Design Plan, land uses have been allocated that will result in a planned community that has its own identity. Having a stacked townhouse development located at a minor point of entry to the community is a means of achieving this. A variety of residential and non-residential land uses to the interior creates a distinct community.

2. To define quality public and private spaces through development.

Each of the units will have its own private space in the form of balconies and porches that provide an outdoor amenity space. The subdivision includes parkland and open space areas

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and the abutting property to the west is a City Park, so the public realm is well addressed in the Riverside South Community Design Plan.

3. To create places that are safe, accessible and easy to get to, and move through.

The site abuts roads and sidewalks on two sides, making it accessible and easy to get to. A single driveway provides safe access to the units. The front doors of the buildings have a sidewalk connection to the main driveway for safe accessibility.

4. To ensure that new development respects the character of existing areas.

Buildings are turned 90 degrees to Earl Armstrong Road to have their narrower dimension abutting the road and the bungalow semi-detached homes located some distance across the arterial road.

5. To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.

This is a compact form of development. The buildings have some degree of variety as they vary between having 6 units, 8 units and 10 units.

6. To understand and respect natural processes and features in development design.

There are no significant natural features on the site that require protection or preservation. Stormwater runoff is released in a controlled manner into the City's stormwater management system so as not to cause damage to downstream areas.

7. To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.

The site is adjacent to sidewalks for walking and is within walking distance of Spratt Road where OC Transpo Route 99 provides daily transit service.

Section 4.11 addresses urban design and compatible development. The following commentary is provided with respect to the design issues raised in Section 4.11;

The site is not a high-rise building nor is it in proximity to the Parliament Buildings, so there are no issues to be addressed with respect to the protection of views in this development.

In terms of building design, compatibility is achieved with adjacent development through various means:

All proposed stacked townhouse buildings have a large separation distance from the existing Fire Station on the adjacent property to the west and from the bungalow semi-detached homes across Earl Armstrong Road. The front façade of the buildings is articulated and the roofs are not

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flat. The end units of buildings have the front exterior materials wrap around the side elevation of the buildings.

As noted previously, the massing of the buildings is such that the narrow ends of the building abut Earl Armstrong Road. The buildings are a modest four storeys which is compatible with the abutting developments.

The rooflines are articulated as are all sides of the proposed buildings. All buildings have either front porches for the lower units or front balconies for the upper units of the front facades. Windows are large and expansive to let in light to the units and even the front doors have transoms to allow additional light to enter the units.

The stormwater management pond in front of Block 5 provides a transition in building form abutting Ralph Hennessy Avenue between Block 1, the stacked townhouse building located at the corner of Ralph Hennessy Avenue and Earl Armstrong Road and the conventional townhouse units recently constructed on Carnelian Crescent to the south. The stormwater management pond is not intended as an amenity area to the site or to the subdivision. However, it does provide a form of visual relief for the subdivision in which most of the units are planned to be built close to the road.

The site is located at the intersection of an arterial road and collector road that serves as a Neighbourhood Gateway in the approved Community Design Plan. As encouraged in the Official Plan, the building at the street intersection is located close to the street (3m). Brick on the front street level of the lower units is wrapped around the corner so that the side elevation of the end unit abutting Earl Armstrong Road is also clad in brick.

As a condominium project, all units have elevated front porches that lead by steps to the street and to the internal sidewalk system. The upper level units each have a front balcony abutting the living room and a balcony abutting the master bedroom at the rear of the unit.

This site is not a designated Design Priority Area in the Official Plan.

Further commentary on design as part of this Design Brief is included in Section 3.3 Riverside Community Design Plan. The updated Community Design Plan was approved in June of 2016 and incorporates the relevant aspects of the Building Better and Smarter Suburbs initiative. Also, Section 4.0 addresses how the project design meets the City's Urban Design Guidelines for Greenfield Neighbourhoods.

Schedule E to the Official Plan, Urban Road Network designates Earl Armstrong Road as an Arterial Road and Ralph Hennessy Avenue as a Proposed Collector. In addition, Earl Armstrong Road is shown as a City-wide Route as part of the City's Multi-Use Pathways, On-Road Connections on Schedule I, Multi-Use Pathways and Scenic Routes (Urban). Figure 4 is an excerpt of Schedule E Urban Road Network and Figure 5 is an excerpt of Schedule I.

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Figure 4 – City of Ottawa Official Plan, Schedule E, Urban Road Network.



Figure 5 – City of Ottawa Official Plan, Schedule I, Multi-Use Pathway and Scenic Routes (Urban).

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The site is within the Airport Vicinity Development Zone of the MacDonald-Cartier Airport on Schedule K Environmental Constraints as shown on Figure 6 below. A Noise Control Feasibility Study has been prepared regarding potential aircraft noise and mitigation measures.

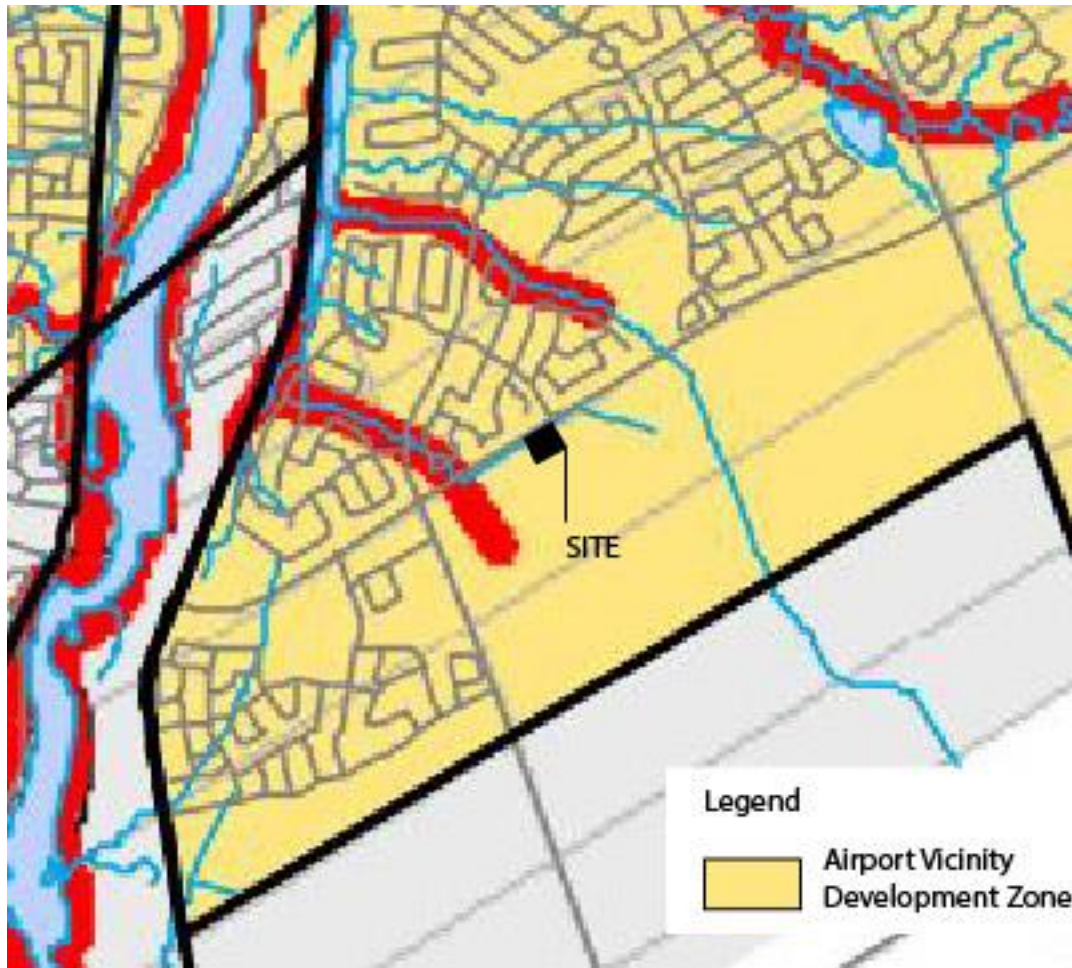


Figure 6 – City of Ottawa Official Plan, Schedule K, Environmental Constraints.

Annex 1 to the Official Plan states that arterial roads are the major roads of the City that carry large volumes of traffic. They function as major public and infrastructure corridors and accommodated car and truck traffic while serving pedestrians, public utilities, cyclists and public transit buses.

According to OC Transpo's Route Map that forms Figure 7, the site is presently serviced daily by Local Route 99 at the intersection of Earl Armstrong Road and Spratt Road. This Route provides service to the Downtown.

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Figure 7 – OC Transpo Route Map

Section 2.3.2 of the Official Plan addresses water and wastewater services. It is Official Plan policy (Section 2.3.2.1) that development in Public Service Areas (the urban area) have both public water and wastewater. That is what is proposed.

In conclusion, the site plan is in conformity with the City of Ottawa Official Plan.

3.3 RIVERSIDE SOUTH COMMUNITY DESIGN PLAN

The site is within the limits of the Riverside South Community Design Plan (CDP) that was updated and approved by City Council on June 22, 2016. One of the most significant changes from the previously approved CDP is the provision of a rapid transit corridor and higher density housing concentrated around the corridor.

The CDP is a long term plan that enables Riverside South Community to grow from about 5,000 units and 14,000 residents in 2014 to ultimately about 20,500 units and 55,000 residents. Land is provided for up to 27,000 jobs to be accommodated in the CDP. The CDP is also the first CDP to implement the Building Better and Smarter Suburbs (BBSS/IR) initiative by the City.

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The CDP also incorporates Transit-Oriented Development principles such as locating higher density development within 600m of a rapid transit corridor. This site is beyond 600m from the corridor but is located on Earl Armstrong Road which is the major east-west arterial road within the Community.

The key objectives of the CDP that are relevant to this application for Site Plan Control on Block 211 are:

- To create land use and road patterns that support various modes of transportation
- To establish a range of residential densities and foster a mix of unit types
- To ensure consistent treatment of buildings, street edges, boulevards, landscape areas and open spaces

The Riverside South Community Design Plan embraces the principles of Transit-Oriented Development (TOD). Although this site is outside the 600m radius of a Rapid Transit Stop, the CDP provides for pockets of medium density residential to locate at the intersection of collector and arterial roads to provide a variety of built form in the Community. Such is the case with this site.

Section 2.2.3 of the CDP sets out the road hierarchy. Earl Armstrong Road is an important east-west arterial road that is connected to Strandherd Drive, west of the Rideau River via Vimy Memorial Bridge. The CDP plans for its easterly extension to Bank Street. Ralph Hennessy is a collector road that will serve as a connecting link between Earl Armstrong Road and a planned Rapid Transit stop to the south. The road allowance has a 30m wide right-of-way where it abuts this site.

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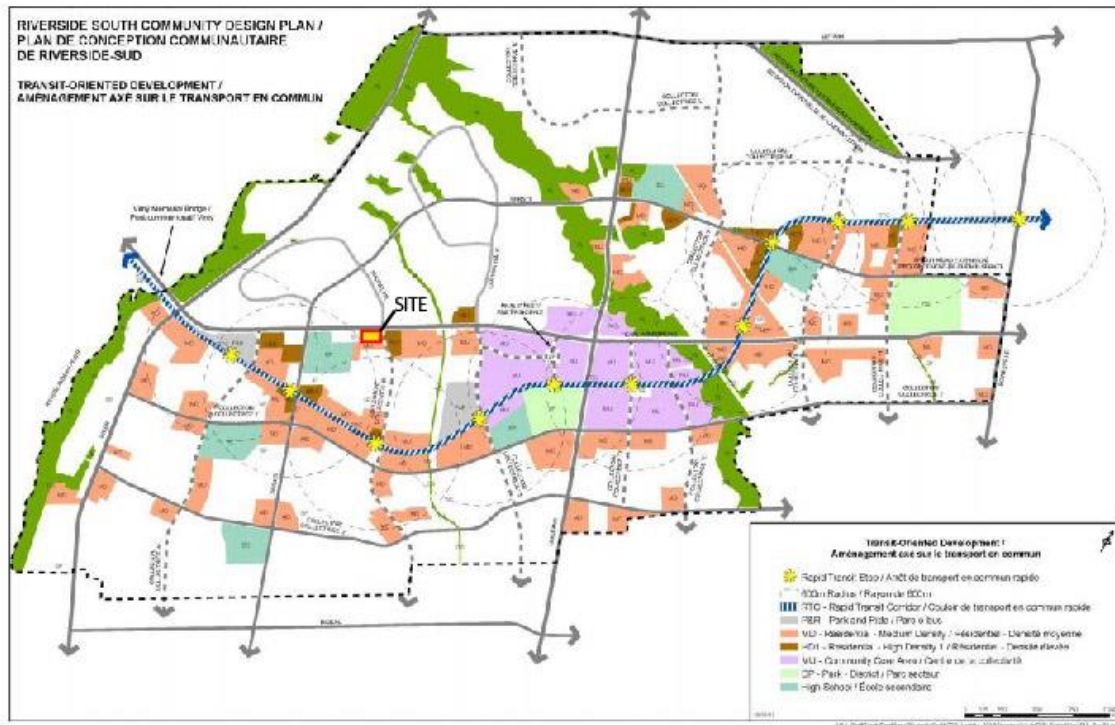


Figure 8. Transit-Oriented Development Map

Figure 8 is the Transit-Oriented Development Map to the Riverside South CDP. Block 211 is designated as High Density 1 on the Land Use Schedule. Section 2.3.1 Density Distribution, advises that High Density 1 sites are mainly for ground-oriented stacked townhouse units that may be located in the Community Core, near transit stations and near major roads. In this case, the site is adjacent to a major road. The planned density of development is an average net density of 60 units per hectare to achieve the Official Plan density target of 29 units per net hectare for ground-oriented units that include single detached, semi-detached and conventional townhouse units. The average net density of Block 211 is 62 units per net hectare (68 units on 1.1ha of land).

Section 3 of the Riverside South CDP contains Community Design and Streetscape Guidelines.

Section 3.1.1 speaks to creating a community identity. Fencing along arterial and collector roads and street lighting are to be used to reinforce the Community's distinct character. The type and location of street trees may also be used to create a neighbourhood identity.

Fencing on arterial roads is to be visually interesting and allow for an edge for planting. Rear lotting has been avoided on Block 211 abutting Earl Armstrong Road. Instead, townhouse block ends with brick on the lower elevation have been used as an alternative. This is similar in design

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to the project recently built by Urbandale Construction to the west on Earl Armstrong Road at the intersection with Brian Good Way.

The landscape plan submitted as part of this application shows the proposed landscape treatment including tree planting in the abutting arterial and collector road allowances and fencing. These elements are used to assist in creating a community and neighbourhood identity.

Section 3.2 is about streetscape guidelines. Earl Armstrong Road is a four-lane divided arterial road with a cycling lane and a dedicated right-turn lane abutting Block 211. The road allowance includes a concrete sidewalk adjacent to the curb and a landscaped boulevard. Ralph Hennessy Avenue is 30m wide collector road and will include a concrete sidewalk and a landscaped boulevard.

The site plan shows that the stacked townhouse buildings abutting Ralph Hennessy Avenue are oriented to the street and have functional porches. The garages for the units are at the rear of the building and are not visible from the street.

For both abutting road allowances, the boulevards will be planted with street trees and sodded. Pedestrian scale street lighting will be used on the collector road.

The intersection abutting the site is a Neighbourhood Gateway. There is an existing planting bed at the intersection in the road allowance and additional planting is proposed on-site adjacent to the planting bed.

Section 6.0 Site and Built Form Guidelines addresses different housing types and other land uses. There are no design guidelines specifically for stacked townhouse units such as is planned for this site. However, there are design guidelines for street townhouse dwellings which is the closest form of housing to stacked townhouse units. The following is a commentary on the project design and the street townhouse design guidelines;

Buildings are encouraged to have a consistent distance from the street. The proposed building at the corner of Ralph Hennessy Avenue and Earl Armstrong Road does achieve a consistent distance from the street. The building setback from Ralph Hennessy Avenue to the front face of the building is about 6m, so there is privacy afforded to the residents and adequate space for landscaping.

The second building facing Ralph Hennessy Avenue is setback further from the street in order to accommodate a stormwater management pond.

It is necessary to locate the stormwater management pond adjacent to Ralph Hennessy Avenue as the stormwater management design for the site is based on the criteria outlined in the JL Richards Riverside South Phase 8 Servicing Report and design practices outlined in the City of Ottawa Sewer Design Guidelines. The minor system capture rate is restricted to 79 L/s, 100-year major system flows must be stored on site, and there shall be adequate emergency overflow conveyance off-site to the City ROW. The very restrictive minor system criteria require

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major system storage for events less than the 2-year storm. In order to minimize water ponding in the driving and parking areas, the site was graded to direct stormwater to the dry pond at the front of the site along Ralph Hennessy Avenue. Emergency spillover from the pond will be directed to Ralph Hennessy Avenue.

The front entries are visible from the street. Side elevations visible from the street carry the architectural treatment from the front elevation. A variety of window sizes are used on the front elevation. The townhouse buildings have cohesive rooflines, window and entry placement.

In conclusion, the use of the site for a stacked townhouse development and the project design is in conformity with the recently approved Riverside South Community Design Plan.

4.0 URBAN DESIGN GUIDELINES FOR GREENFIELD NEIGHBOURHOODS

The Urban Design Guidelines for Greenfield Neighbourhoods was adopted by Council in September 2007. It provides general guidelines for the design of subdivisions in greenfield settings. Since the site is within a registered plan of subdivision, this design commentary will not comment on neighbourhood or street design guidelines as that evaluation and approval has already occurred.

However, Section 3 addresses residential building and site design and commentary is provided here as to how the site design responds favourably to the relevant guidelines of this Section.

Guideline 34 speaks to locating residential buildings close to the street with the front face of the building addressing the street while making room for landscaping. It encourages varied setbacks and projections.

The site plan meets Guideline 34 as the front face of the building abutting Ralph Hennessy Avenue is about 6m from the front lot line. The front porches are even closer at about 4.6m while leaving sufficient space for landscaping. The front façade is articulated to give visual interest.

Guideline 37 encourages design where windows and doors are prominent features that address the streets they front.

The units facing Ralph Hennessy Avenue have large windows and doors. The doors and windows on the front elevation have transoms to increase their visibility and address numbers are placed above the door. The windows are large on the front elevation to let light into the unit and give greater prominence.

Guideline 39 calls for porches that are large enough to accommodate sitting areas into the building design. All units have porches that are large enough to have sitting areas.

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Guideline 42 speaks to the location of surface parking. All 68 units have in-unit garages and the only surface parking on-site is 15 visitor spaces. Twelve of the 15 spaces are not between the proposed buildings and the public street. The 12 parking spaces are at locations where the site is raised in elevation and therefore, they are not visible from the street.

Guideline 44 calls for residential buildings to be designed so that garages do not dominate the width of the front façade. All buildings will have garages at the rear of the units so there will not be any space on the front facades of the eight proposed buildings occupied by garages.

In conclusion, the site design meets the relevant design guidelines of the City's Urban Design Guidelines for Greenfield Neighbourhoods.

5.0 ZONING BY-LAW

The site is regulated by City of Ottawa By-law 2008-250. The site is zoned Residential Fourth Density R4Z Zone as shown on Figure 9. This Zone allows a wide range of housing types of up to four storeys in height. The R4Z subzone that applies to this site is intended for developing communities such as Riverside South to promote efficient land uses, a compact building form while highlighting newer approaches to urban design.

Stacked dwellings are a permitted use as is a planned unit development. The site plan complies with the development standards of the R4Z Zone including parking and visitors parking.

The site plan has been designed to comply with the Zoning By-law. A summary of relevant Zoning standards is provided on the site plan.

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6.1 PHASE 1 ENVIRONMENTAL SITE ASSESSMENT



6.2 GEOTECHNICAL LETTER

Golder Associates provided a letter dated January 12, 2017 confirming that the geotechnical recommendations provided in their report to Riverside South Development Construction titled, "Geotechnical Investigation, Proposed Residential Development, Riverside South Development (Phase 8), Ottawa, Ontario" dated July 2015 (Report Number 1418804) are applicable to the design and construction of housing in Block 211. The recommendations of this Report have previously been accepted by the City through the subdivision review and approval process.

6.3 SERVICING AND STORMWATER MANAGEMENT BRIEF

Stantec prepared a Site Servicing and Stormwater Management Report in support of this application.

Potable water is supplied to the site by two watermain connections to a 300mm diameter watermain on Ralph Hennessy Avenue. The hydraulic analysis concluded that the proposed water service network is capable of servicing the development and meets all servicing requirements of the City under typical demand conditions as well as emergency fire demand conditions.

The site will be serviced through the 450mm diameter sanitary sewer on Ralph Hennessy Avenue. The proposed development will generate a total sanitary peak flow of about 3.3L/s which is above the flow forecast for the site in the master servicing study for the subdivision, but a negligible increase nevertheless. The preferred cover requirement of 2.5m for the sanitary sewer system will be satisfied in all locations and requirements for slope and velocities have been met within the proposed sewers.

The lot grading has been designed to allow for the major overland flow to outlet in accordance with the recommendations of the subdivision's master servicing study. It also adheres to the grade raise restrictions recommended by the geotechnical investigation. However, a small area in the west edge of the site exceeds the maximum allowable grade raise and the proposed grading plan has been provided to a geotechnical engineer for review and recommendation.

The site storm sewer system will connect to a 1350mm diameter storm sewer on Ralph Hennessy Avenue that will ultimately connect to the existing Riverside South Pond 1. Quality control of stormwater will be provided by that Pond. Detailed site grading directs overland flows to a stormwater management dry pond with an emergency overflow to Ralph Hennessy Avenue. The dry pond will intercept major system overflows from the site and will fully contain 100-year overflows and discharge back into the minor system. A minor area surrounding the site cannot be graded to enter the site storm system and therefore, will sheet drain uncontrolled off-site.

In summary, the proposed servicing of the site meets City of Ottawa design requirements and the design guidelines of the Ministry of the Environment and Climate Change.

6.4 ENVIRONMENTAL NOISE ASSESSMENT REPORT

Stantec prepared an Environmental Noise Assessment Report that took into consideration both the level of noise related to Macdonald Cartier International Airport flight activity as well as noise from the abutting arterial road, Earl Armstrong Road. The site is within the Airport Vicinity Development Zone and outside the NEF 35 contour for the airport.

The Report applied the noise level standards of the Ontario Ministry of the Environment and Climate Change NPC-300 to the site in conjunction with the City of Ottawa's Environmental Noise Control Guidelines dated January 2016 to determine the extent to which noise levels will be of concern to future residents.

For the four proposed buildings abutting Earl Armstrong Road, forced air heating and central air conditioning are required to be installed in all units and an appropriate noise warning clause included in all agreements of purchase and sale. For the four interior buildings abutting the rear of lots on Carnelian Crescent, forced air heating and provision for adding central air condition is required for all units and an appropriate noise warning clause included in all agreements of purchase and sale.

In order to address noise levels related to aircraft noise, appropriate building materials such as double glazed windows are to be incorporated into the building design and an appropriate warning clause is to be included in all agreements of purchase and sale.

The application of these recommended measures will allow the residential development to proceed in accordance with MOECC criteria and City of Ottawa Environmental Noise Control Guidelines.

7.0 CONCLUSION

The application for Site Plan Control by Urbandale Construction for a 68-unit stacked townhouse development on Block 211, Plan 4M-1573 has been fully reviewed from a land use planning perspective.

It is our professional planning opinion that the application for Site Plan Control should be approved for the following reasons;

1. The approval of the application is consistent with the Provincial Policy Statement 2014.
2. The site plan is in conformity with the City of Ottawa Official Plan.
3. The site plan is in conformity with the Riverside South Community Design Plan and its Design Guidelines.

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4. The site plan is in conformity with the Zoning By-law.
5. The site plan, landscape plan and elevation drawings meet the relevant Urban Design Guidelines for Greenfield Neighbourhoods.
6. The site plan represents good land use planning.

Respectfully submitted,



David Krajaefski, MCIP, RPP

Senior Project Manager