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## **Planning Rationale**

# **URBANDALE CORPORATION COWAN'S GROVE JAZZ LITE 4791 BANK STREET**





## **Table of Contents**

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1.0	Introduction .....	1
1.1	Purpose.....	1
1.2	Site Context.....	2
1.2.1	Location and Description .....	2
1.2.2	Community Context .....	2
2.0	The Proposal .....	4
2.1	Site Plan Control Application .....	4
3.0	Policy Context .....	5
3.1	Planning Act (R.S.O. 1990, Chapter P.13) .....	5
3.2	Provincial Policy Statement (2014).....	6
3.3	The City of Ottawa Official Plan.....	6
3.3.1	Urban Design Guidelines .....	7
3.4	City of Ottawa Zoning By-law 2008-250 .....	7
3.5	Background Studies .....	9
4.0	Conclusion .....	9

## **Table of Appendices**

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Appendix A	Reduced Site Plan
Appendix B	Reduced Building Elevations
Appendix C	Review of Planning Act
Appendix D	Review of Provincial Policy Statement (2014)
Appendix E	Review of City of Ottawa Official Plan (including OPA #196 and #150, #180 as they have been Council Approved)
Appendix F	Review of the City of Ottawa Zoning By-law (2008-250)



# Planning Rationale

## Urbandale Corporation, 4791 Bank Street

### Cowan's Grove Commercial Plaza

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## 1.0 Introduction

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J.L. Richards & Associates Limited (JLR) has been retained by Urbandale Corporation to prepare a Planning Rationale in support of a Site Plan Control application for the property located at 4791 Bank Street (known as the Cowan's Grove Jazz Lite) in the Former Township of Osgoode, City of Ottawa as shown on Figure 1. This application seeks to permit the development of a planned unit development, consisting of 102 residential units distributed between 7 stacked townhouse blocks located along a private internal road network accessed via Bank Street and Longworth Ave, as shown on Figure 2. A larger plan can be found in Appendix 'A'.



**Figure 1 – Aerial view of Cowan's Grove Jazz Lite**

### 1.1 Purpose

This Report will demonstrate how the development will be consistent with the Provincial Policy Statement, and is in conformity with the City of Ottawa Official Plan. This rationale also demonstrates that this proposal conforms to the current Zoning By-law.

# **Planning Rationale**

## **Urbandale Corporation, 4791 Bank Street**

### **Cowen's Grove Commercial Plaza**

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The layout of the proposed development will provide logical access to surrounding residential, commercial services and institutional uses as well as ensure multimodal connectivity that meets the needs of both this growing community and the City as a whole.

#### **1.2 Site Context**

##### **1.2.1 Location and Description**

The legal description of the Subject Site is Part Lot 19, Concession 5 Rideau Front (Gloucester), in the Former Township of Osgoode, now the City of Ottawa. The entire Subdivision property is approximately 29 ha in size while the land subject to this application is approximately 1.39 ha in size. This block is situated on the south west side along Bank Street.

As shown in Figure 1, the site currently consists of undeveloped urban lands. These lands are have recently been redesignated to General Urban Area and rezoned to General Mixed Use (GM) to allow for a range of uses including but not limited to: a low-rise apartment dwelling, a mid-rise apartment dwelling, a planned unit development, a stacked dwelling and a townhouse dwelling.

##### **1.2.2 Community Context**

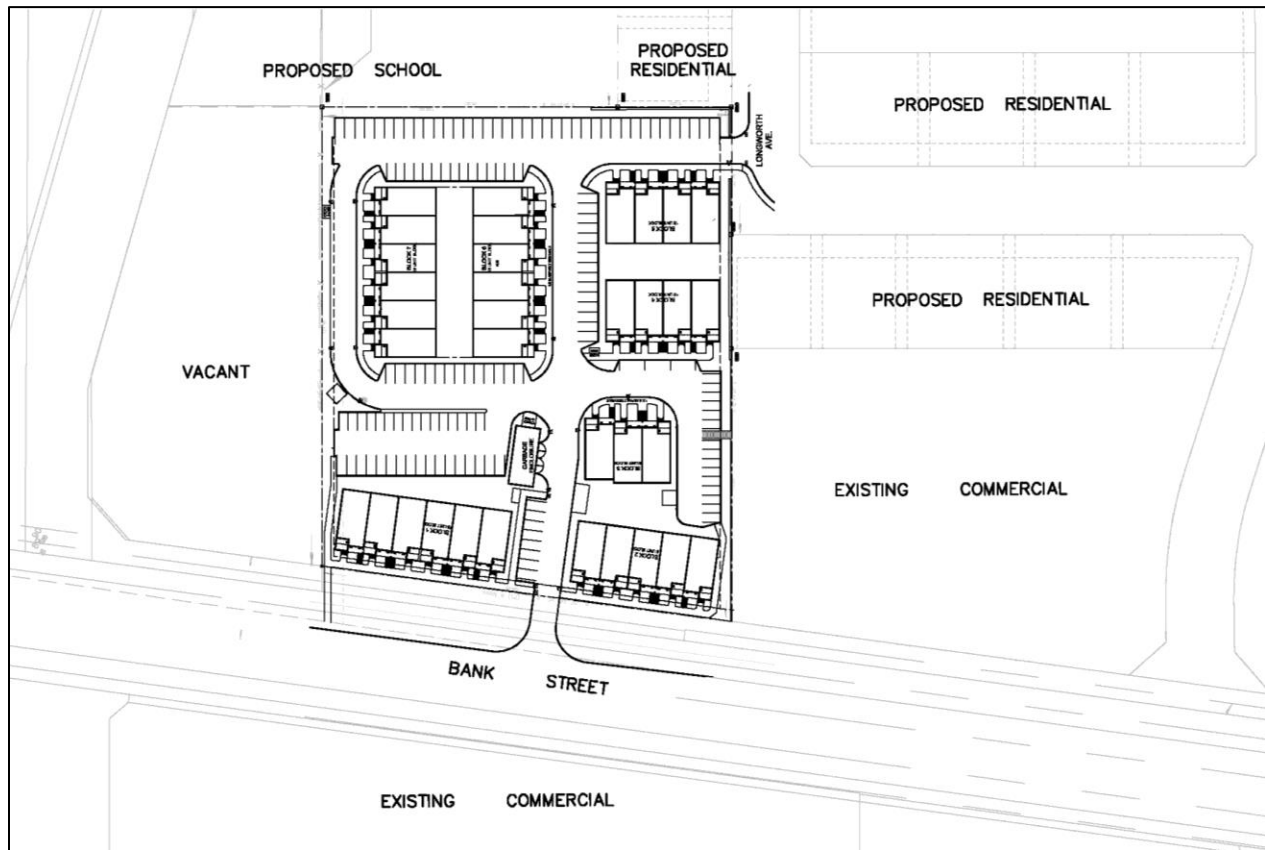
The Subject Site is located within the Leitrim/Findlay Creek Community. An existing commercial plaza is located on the opposite side of Bank Street which is surrounded with low and medium density residential development, as shown on Figure 2. Low and medium density residential exists for the lands immediately to the east and southeast of the Subject Site and on the remainder of the Cowan's Grove Subdivision lands. A commercial plaza is located immediately to the south west.

# Planning Rationale

## Urbandale Corporation, 4791 Bank Street

### Cowen's Grove Commercial Plaza

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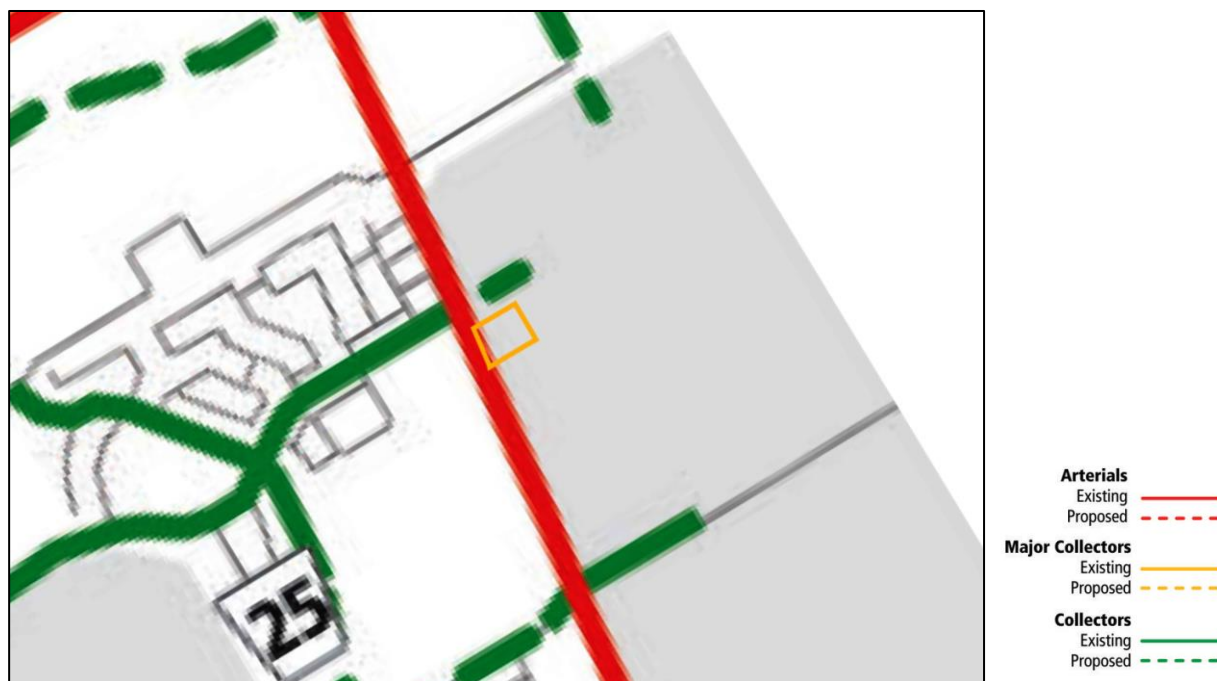


**Figure 2 – Surrounding Context: Cowan's Grove Jazz Lite**

The Subject Site will be located within close proximity to two schools; Vimy Ridge Public School and a proposed school block which has been accommodated for in the Subdivision development to reflect the requirements of the French Catholic School Board.

There are also two City parks proposed within the Draft Subdivision to the east of the site.

Both Bank Street and Leitrim Road are arterial roads in the City that are heavily used by residents traveling to and from the Leitrim/Findlay Creek Community (see Figure 3).



**Figure 3 – Existing City of Ottawa Official Plan Schedule E: Urban Road Network**

The site is serviced by OC Transpo which operates around the site. There are currently two service routes available: Route 294 and Route 93.

The existing connectivity of the site is also well developed. Routes provide a beneficial feature for pedestrians and cyclists traveling nearby. There are existing sidewalks on the west side of Bank Street and are proposed along the east side, both of which are currently well lit. A southbound bike lane exists along Bank Street, and a paved shoulder exists northbound along Bank Street. In addition, the intersection at Bank Street and Shuttleworth Drive has existing crosswalk signals which promote road safety.

Improvements to and road widening of Bank Street in order to accommodate the Cowan's Grove Subdivision along with other adjacent developments are also proposed. Details can be found in the accompanying Transportation Impact Assessment (TIA) Report.

## **2.0 The Proposal**

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### **2.1 Site Plan Control Application**

Appendix 'A' is the site plan for the proposed development. This planned unit development consists of 7, three storey residential buildings. The breakdown for the number of units per block is as follows:



**Planning Rationale**  
**Urbandale Corporation, 4791 Bank Street**  
**Cowen's Grove Commercial Plaza**

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<b>Proposed Use</b>	<b>Units Per Block</b>
Block 1	18
Block 2	15
Block 3	9
Block 4	12
Block 5	12
Block 6	18
Block 7	18
<b>Total Number of Dwelling Units</b>	<b>102</b>

Proposed building elevations can be found in Appendix 'B'.

Trunk municipal water, sanitary sewer and stormwater sewer services are available in Bank Street to service the Subject Site. A site servicing plan and report accompanies this submission.

As shown in Appendix 'A', vehicular access to the Subject Site's private internal street will be from both Bank Street and Longworth Avenue. The intersection of this private street and Bank Street will not be signalized.

The proposed site will also include:

- 51 bicycle parking spaces
- 142 outdoor parking spaces (46 of which are for small cars only)
- internal private streets (minimum widths of 6.7 m)
- 1 garbage enclosure to serve all four buildings (designed in accordance with Section 110 of Zoning By-law)

### **3.0 Policy Context**

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#### **3.1 Planning Act (R.S.O. 1990, Chapter P.13)**

Section 41 (4) of the *Planning Act* provides details regarding the development criteria to be considered with a Site Plan Control application. Please see Appendix 'C' for the detailed review of the Section 41 development criteria.

The proposed Site Plan for the Cowan's Grove Jazz Lite development has addressed all of the criteria identified in the Act. This rationale confirms our opinion that the proposed site plan has met the criteria identified in the Act and is appropriate for development.

Based on the assessment of this rationale, the application for site plan control for the Subject Site meets the policies of the Planning Act, is appropriate and represents good land use planning.

### **3.2 Provincial Policy Statement (2014)**

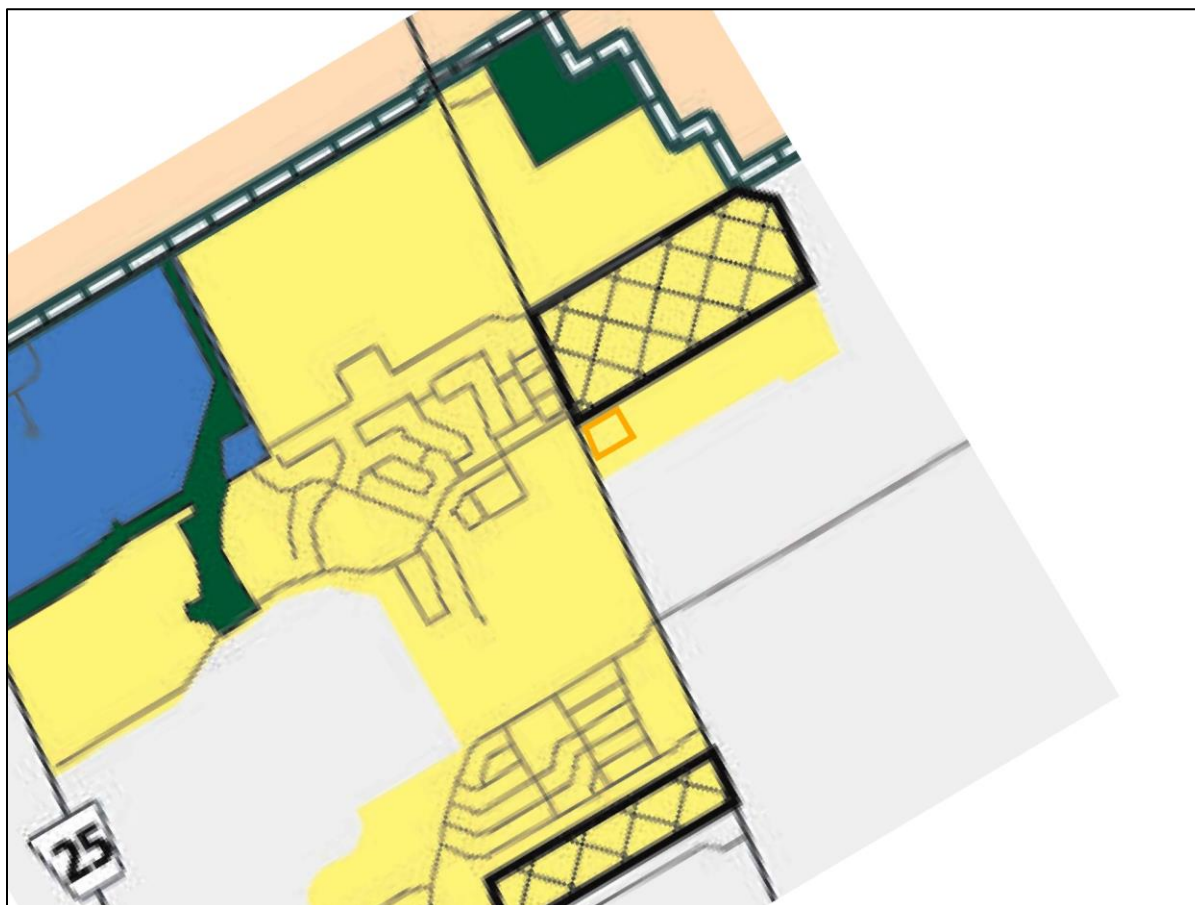
This Site Plan is consistent with the vision and ideas of the Provincial Policy Statement (PPS) as issued under Section 3 of the *Planning Act*. For example, Section 1.1 of the PPS focuses on promoting efficient land use and development patterns that occur adjacent to built-up areas and which provide opportunities for intensification. It is our opinion that this proposal is consistent with the PPS, 2014. Please see Appendix 'D' for the detailed review of the relevant PPS policies.

Based on the assessment of this rationale, the application for site plan control for the Subject Site is consistent with the Provincial Policy Statement, is appropriate and represents good land use planning.

### **3.3 The City of Ottawa Official Plan**

The City of Ottawa Official Plan, adopted by City Council in May 2003, has been updated and amended numerous times by both Council and the Ontario Municipal Board. For the purposes of this Planning Rationale, the on-line consolidated version of the Official Plan, including Amendment #150 and #180, were used as reference along with Official Plan Amendment (OPA) #196.

The Subject Site is located within the urban boundary of the City of Ottawa and is part of the settlement area for the City. Schedule 'B' of the Official Plan shows the site designated as a General Urban Area (refer to Figure 4) within the Urban Area. This form of development is identified as a part of the General Urban Area.



**Figure 4 – Existing City of Ottawa Official Plan Schedule B: 'General Urban Area'**

### **3.3.1 Urban Design Guidelines**

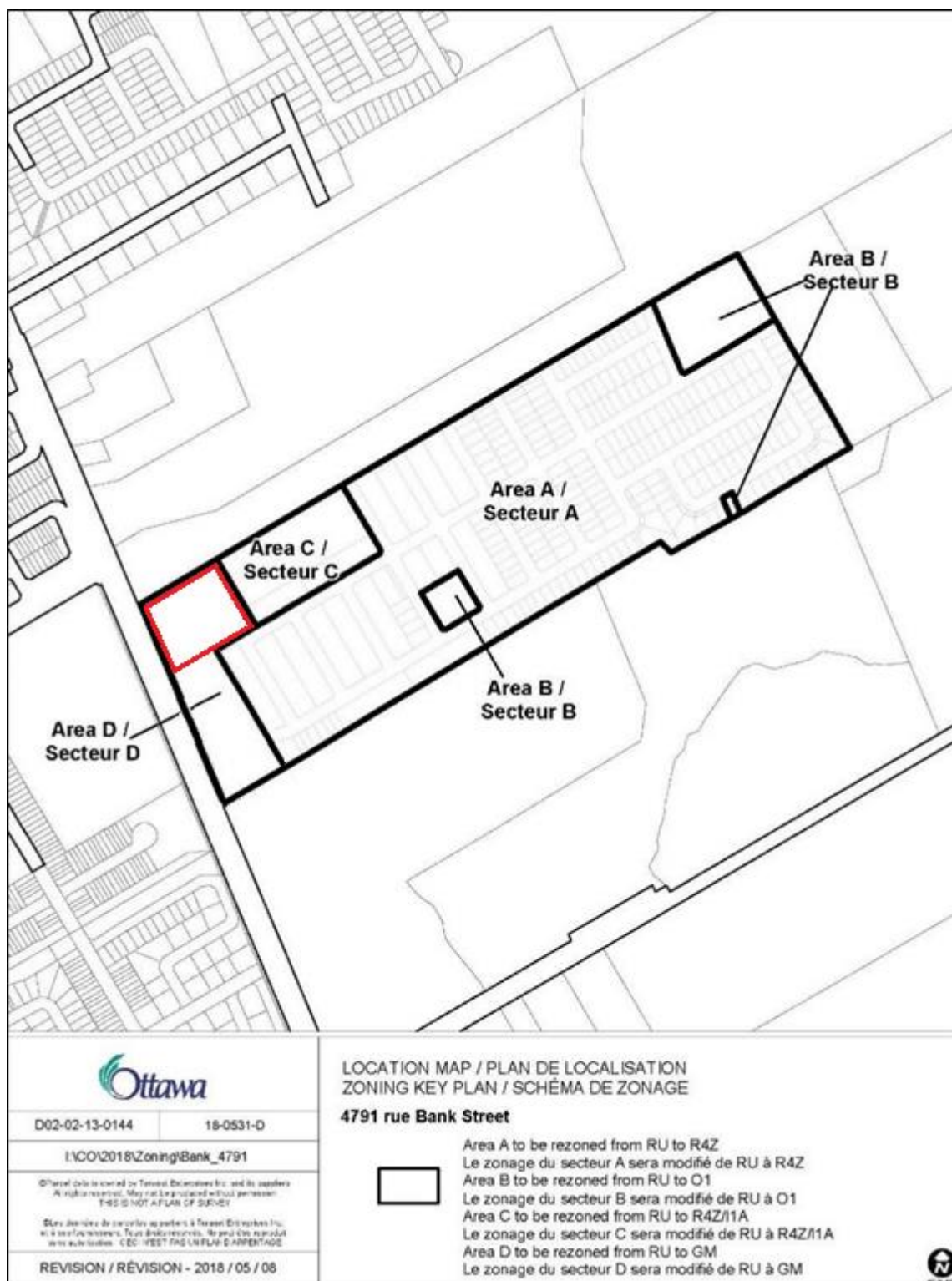
The proposed Site Plan was created with special consideration for the policies of the General Urban Area with emphasis on the policies for design, and compatibility. Please see Appendix 'E' for the detailed review of the relevant Official Plan policies.

Based on the assessment of this rationale, the application for site plan control for the Subject Site conforms to the policies of the City of Ottawa Official Plan and relevant design guidelines, is appropriate and represents good land use planning

### **3.4 City of Ottawa Zoning By-law 2008-250**

On June 27<sup>th</sup>, 2018 the Subject Site was rezoned to GM – General Mixed Use in the City of Ottawa Comprehensive Zoning By-law 2008-250 by By-law #221-2018.

**Planning Rationale**  
**Urbandale Corporation, 4791 Bank Street**  
**Cowen's Grove Commercial Plaza**



**Figure 5 – City of Ottawa Zoning By-law 2008-250: Cowan's Grove Jazz Lite Development**

## **Planning Rationale**

### **Urbandale Corporation, 4791 Bank Street**

### **Cowen's Grove Commercial Plaza**

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The proposed Site Plan has been designed to comply with the zoning standards of the GM zone and all other relevant provisions including the General and Parking requirements.

The site plan includes a detailed table demonstrating conformity. Please see Appendix 'F' for the detailed review of the relevant Zoning By-law policies and standards.

Based on the assessment of this rationale, the application for site plan control for the Subject Site meets all the required provisions and standards of the City of Ottawa Zoning By-law, is appropriate and represents good land use planning

### **3.5 Background Studies**

The following detailed background studies have been completed in support of the draft approval and this application:

- Servicing Brief (2020);
- Transportation Impact Assessment (2019);
- Phase 1 ESA (2013); and
- Noise Impact Assessment (2020).

## **4.0 Conclusion**

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This Report has been prepared in support of an application for a Site Plan Control application for the Cowan's Grove Jazz Lite development, which will see a new planned unit development consisting of 7 stacked townhouse blocks housing a total of 102 dwelling units.

1. The current site plan control application is consistent with the Provincial Policy Statement, 2014 as the proposed development is located within a designated Settlement Area, is a complementary use for the neighbourhood, is compatible with its surroundings and is consistent with the provincial interest identified in the Provincial Policy Statement.
2. The application respects the provisions of the Official Plan which has identified these lands as General Urban Area. This proposal conforms to the intent of this designation in that the proposed uses are permitted and are located in an area suitable for their development.
3. The proposed development planned unit development which will consist of 7 stacked townhouse blocks.

Based on the above-noted rationale, the application for site plan control for the Subject Site is appropriate and represents good land use planning.

This report has been prepared for the exclusive use of Urbandale Corporation, for the stated purpose, for the named facility. Its discussions and conclusions are summary in nature and cannot

**Planning Rationale**  
**Urbandale Corporation, 4791 Bank Street**  
**Cowen's Grove Commercial Plaza**

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be properly used, interpreted or extended to other purposes without a detailed understanding and discussions with the client as to its mandated purpose, scope and limitations. This report was prepared for the sole benefit and use of Urbandale Corporation and may not be used or relied on by any other party without the express written consent of J.L. Richards & Associates Limited.

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J.L. RICHARDS & ASSOCIATES LIMITED

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Planner

Reviewed by:



Timothy F. Chadder, MCIP, RPP  
Chief Planner

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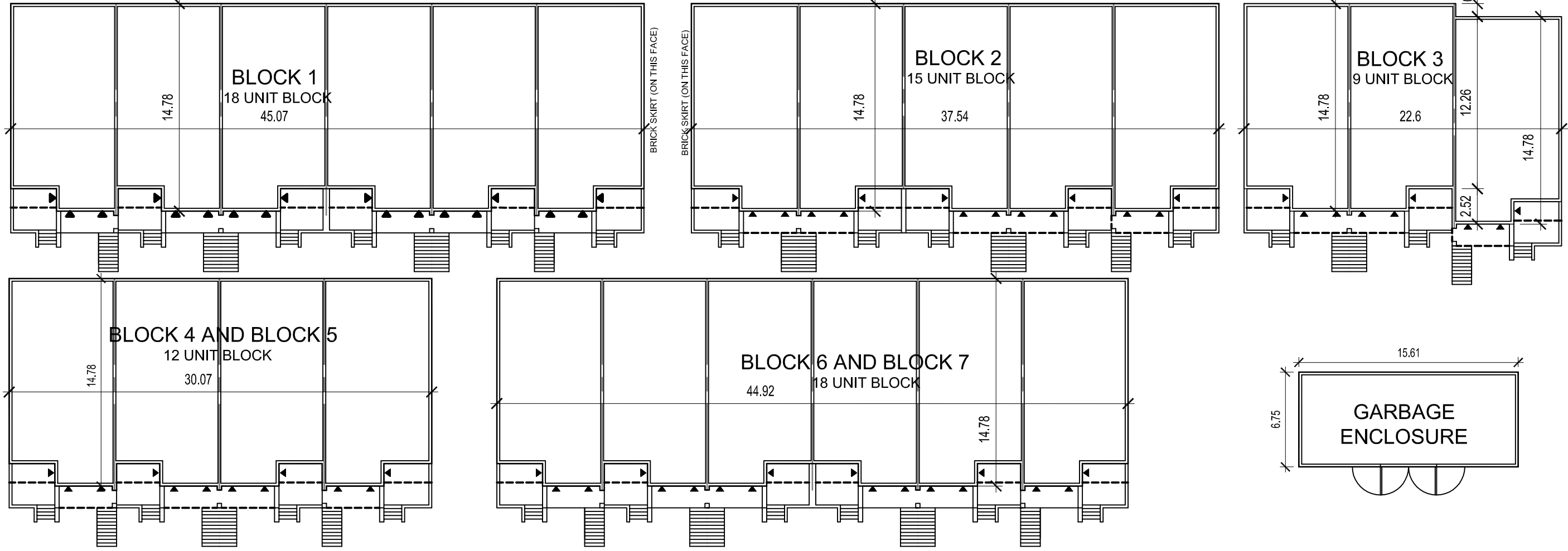
## **Appendix A**

Reduced Site Plan





# BUILDING DIMENSIONS



# BUILDING AREAS

TABLE BELOW FOR INFORMATION FOR CITY OF OTTAWA ZONING BY-LAW 2008-250 CONSOLIDATION

BLOCK NUMBER	UNITS PER BLOCK	GROSS FLOOR AREA	BUILDING AREA
BLOCK 1	18	1788.93 SQ. M.	635.59 SQ. M.
BLOCK 2	15	1487.88 SQ. M.	529.28 SQ. M.
BLOCK 3	9	892.56 SQ. M.	318.88 SQ. M.
BLOCK 4	12	1190.19 SQ. M.	424.23 SQ. M.
BLOCK 5	12	1190.19 SQ. M.	424.23 SQ. M.
BLOCK 6	18	1782.09 SQ. M.	633.62 SQ. M.
BLOCK 7	18	1782.09 SQ. M.	633.62 SQ. M.
GARBAGE ENCLOSURE	N/A	96.6 SQ. M.	105.38 SQ. M.

# ZONING CONFORMANCE TABLE

TABLE BELOW BASED ON GM ZONING, CITY OF OTTAWA ZONING BY-LAW 2008-250 CONSOLIDATION

PERMITTED RESIDENTIAL USES	STACKED DWELLINGS		
TOTAL UNIT COUNT	102 UNITS		
TOTAL FLOORS	3.0 STOREYS		GROUND INSET BELOW GRADE
BUILDING HEIGHT	11 M		
SITE AREA	13873.69 SQ. M.		
TOTAL GROSS FLOOR AREA	10210.53 SQ. M.		
TOTAL BUILDING AREA	3704.83 SQ. M.		
LOT COVERAGE	36.70%		
ASPHALT (DRIVE AISLES AND PARKING SPACES)	5074.49 SQ. M.		
LANDSCAPE AREA	5094.37 SQ. M. (36.7%)		

LANDSCAPE BUFFER LOCATION	REQUIRED	PROVIDED	MINOR VARIANCE REQ.
FRONT YARD (BANK STREET)	3.0 M	3.0 M	NO
REAR YARD	3.0 M	3.0 M	NO
INTERIOR SIDE YARD	3.0 M	3.0 M	NO
EXTERIOR SIDE YARD	3.0 M	3.0 M	NO

ZONING PROVISIONS	REQUIRED	PROVIDED	MINOR VARIANCE REQ.
MINIMUM LOT AREA	NO MINIMUM	13873.69 SQ. M.	NO
MINIMUM LOT WIDTH	NO MINIMUM	108.0 M.	NO
MAXIMUM HEIGHT	18.0 M	11.0 M	NO
FRONT YARD SETBACK	3.0 M	3.0 M	NO
REAR YARD SETBACK	7.5 M	7.5 M	NO
INTERIOR SIDE YARD SETBACK	3.0 M	3.0 M	NO
EXTERIOR SIDE YARD SETBACK	3.0 M	3.0 M	NO

AMENITY SPACE REQUIREMENTS	REQUIRED	PROVIDED	MINOR VARIANCE REQ.
TOTAL AMENITY AREA (SQ. M PER UNIT)	6.0 SQ. M.	16.1 SQ. M. (MIN)	NO
COMMUNAL AMENITY AREA (SQ. M)	306.0 SQ. M.	306 SQ. M.	NO

GENERAL PARKING PROVISIONS	REQUIRED	PROVIDED	
----------------------------	----------	----------	--

PARKING SPACE DIMENSIONS	2.6 M x 5.2 M and UP TO 40 % (SMALL CAR) 2.4 M x 4.6 M, PARALLEL PARKING SPACE 2.6 M x 6.7 M	2.6 x 5.2 M, 2.4 M x 4.6 M and 2.6 M x 6.7 M	
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PARKING SPACE RATES	1.2 x 102 UNITS = 122	142 (INCL. 20 VIST.)	
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PARKING SPACE RATE FOR SMALL CAR (40% ALLOWABLE)	57 SPACES ALLOWED	46 SPACES	
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VISITOR PARKING	0.2 x 102 UNITS = 20	20	
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BICYCLE PARKING	0.5 X 102 UNITS = 51	51	51
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DRIVE AISLE PROVISIONS	6.7 M.	6.7 M.	
FIRE ROUTE PROVISIONS	6.0 M.	6.0 M.	

WINTER SNOW STORAGE		OFF SITE	
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# SYMBOLS AND LEGEND:

	UNIT ENTRANCE
	STANDARD PARKING SPACE
	PARALLEL PARKING SPACE
	SMALL CAR PARKING SPACE
	PROPERTY LINE
	FRONT/SIDE/REAR YARD SETBACKS
	ASPHALT SIDE WALK
	ROAD/CURB
	FIRE ROUTE
	MODULE OF A COMMUNITY MAILBOX STATION
	FIRE ROUTE (NO PARKING) SIGNAGE

CLIENT



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# ISSUES

No.	DESCRIPTION	DATE
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**PROJECT**  
  
####

**PROJECT NO:**  
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SK  
**PROJECT MGR:**  
TERRY BRULE  
**CHECKED BY:**  
  
**APPROVED BY:**  
####

**SHEET TITLE**  
COWAN'S GROVE SITE PLAN

**SHEET NUMBER**  
A-101

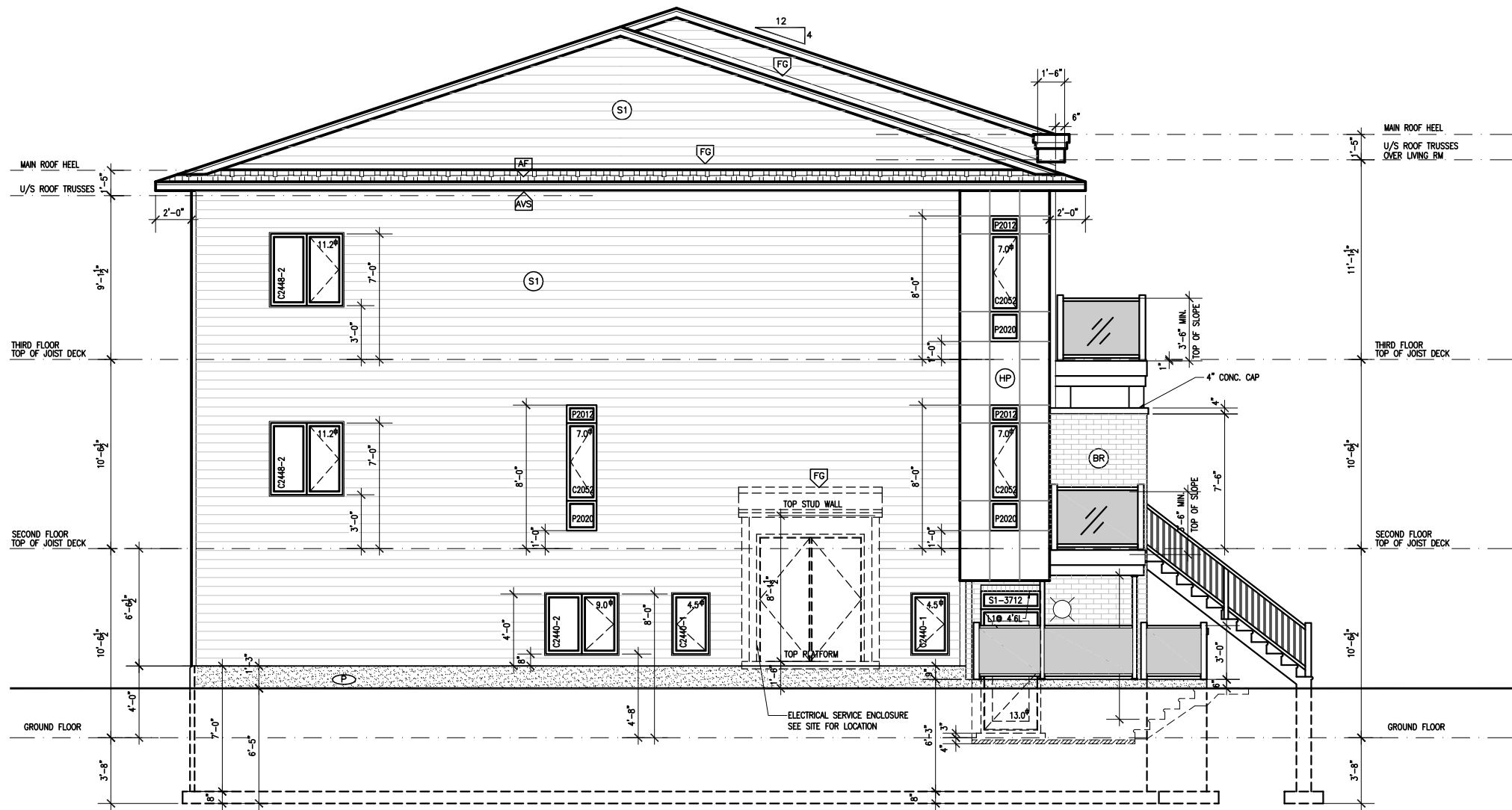
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## **Appendix B**

Reduced Building Elevations






LEFT FRONT ELEVATION

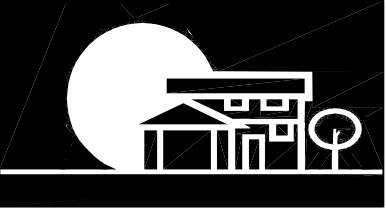
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TOTAL GLAZED OPENING AREA= 74.4 sq.ft. = 5.6%

DWG	REVISIONS	DATE

LASTEST REVISION THIS PAGE:

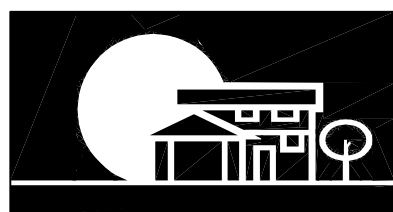


ENERGY STAR



URBANDALE CONSTRUCTION LIMITED  
2193 ARCH STREET, OTTAWA  
"There's Comfort in Higher Standards"

SITE:	COWAN'S GROVE	
MODEL:	JAZZ LITE	
DWG:	GROUP BLOCK 1 LEFT SIDE ELEVATION	
SCALE:	1/8" = 1'-0"	DWG No:
DATE:	JUNE 5,2019	1 OF 4



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2193 ARCH STREET, OTTAWA  
*"There's Comfort in Higher Standards"*

LASTEST REVISION THIS PAGE:
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SITE:	COWAN'S GROVE
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MODEL:

## JAZZ LITE

DWG:

**GROUP BLOCK 1  
FRONT ELEVATION**

SCALE:  
NOT TO SCALE

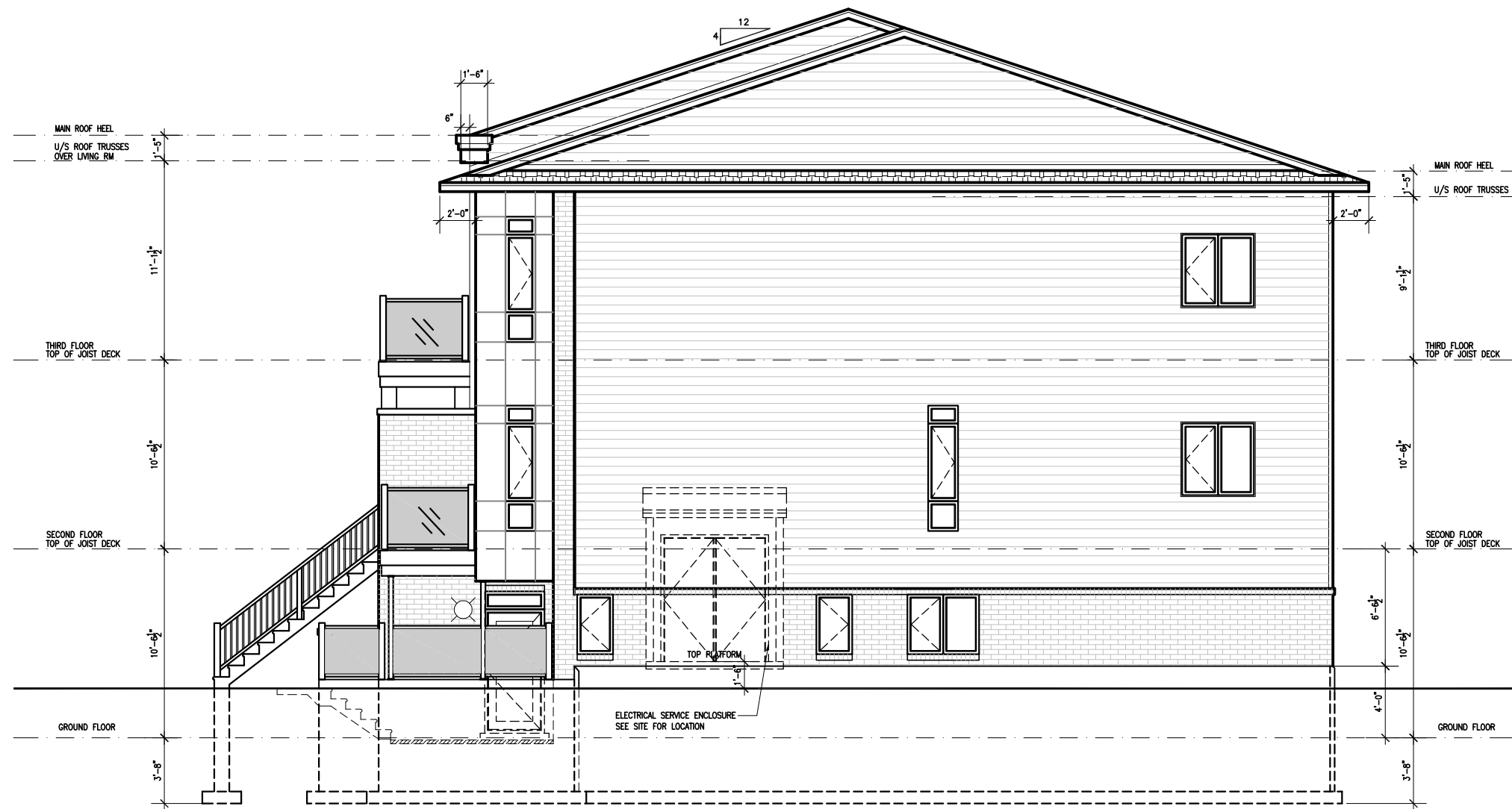
DWG No:
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2

OF 4








**RIGHT FRONT ELEVATION**

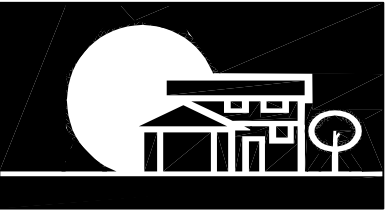
TOTAL EXPOSED BLDG FACE (TEBF)= 1322.05 sq.ft.  
TOTAL GLAZED OPENING AREA= 74.4 sq.ft. = 5.6%

DWG	REVISIONS	DATE

LASTEST REVISION THIS PAGE:



**ENERGY STAR**



URBANDALE CONSTRUCTION LIMITED  
2193 ARCH STREET, OTTAWA  
"There's Comfort in Higher Standards"


SITE:	COWAN'S GROVE	
MODEL:	<b>JAZZ LITE</b>	
DWG:	<b>GROUP BLOCK 1 RIGHT SIDE ELEVATION</b>	
SCALE:	1/8" = 1'-0"	DWG No: <b>3</b>
DATE:	JUNE 5,2019	OF 4





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LASTEST REVISION THIS PAGE:



SITE: COWAN'S GROVE	
MODEL: JAZZ LITE	
DWG: GROUP BLOCK 1 REAR ELEVATION	
SCALE: NOT TO SCALE	DWG No: 4 OF 4
DATE: JUNE 5,2019	

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## **Appendix C**

Review of Planning Act





## **Planning Act, R.S.O, 1990**

Section 41 of the Planning Act provides the legal framework from establishment of Site Plan Control Areas, approvals of plans or drawings and conditions for approval of plans or drawings.

### **Approval of plans or drawings**

*(4) No person shall undertake any development in an area designated under subsection (2) unless the council of the municipality or, where a referral has been made under subsection (12), the Municipal Board has approved one or both, as the council may determine, of the following:*

- 1. Plans showing the location of all buildings and structures to be erected and showing the location of all facilities and works to be provided in conjunction therewith and of all facilities and works required under clause (7) (a), including facilities designed to have regard for accessibility for persons with disabilities.*

The site plan for the Subject Site was designed showing the location of all buildings and structures, the location of all facilities and works to be provided in conjunction therewith and of all facilities and works.

- 2. Drawings showing plan, elevation and cross-section views for each building to be erected, except a building to be used for residential purposes containing less than twenty-five dwelling units, which drawings are sufficient to display,*
  - (a) the massing and conceptual design of the proposed building;*
  - (b) the relationship of the proposed building to adjacent buildings, streets, and exterior areas to which members of the public have access;*
  - (c) the provision of interior walkways, stairs, elevators and escalators to which members of the public have access from streets, open spaces and interior walkways in adjacent buildings;*
  - (d) matters relating to exterior design, including without limitation the character, scale, appearance and design features of buildings, and their sustainable design, but only to the extent that it is a matter of exterior design, if an official plan and a by-law passed under subsection (2) that both contain provisions relating to such matters are in effect in the municipality;*
  - (e) the sustainable design elements on any adjoining highway under a municipality's jurisdiction, including without limitation trees, shrubs, hedges, plantings or other ground cover, permeable paving materials, street furniture, curb ramps, waste and recycling containers and bicycle parking facilities, if an official plan and a by-law passed under subsection (2) that both contain provisions relating to such matters are in effect in the municipality; and*

- (f) facilities designed to have regard for accessibility for persons with disabilities. R.S.O. 1990, c. P.13, s. 41 (4); 2002, c. 9, s. 56 (1); 2006, c. 23, s. 16 (3, 4); 2009, c. 33, Sched. 21, s. 10 (9).*

The elevation drawings found in Appendix "B" incorporate all of the requirements of Section 41 (4)(2) of the Planning Act.

**Conditions to approval of plans**

*(7) As a condition to the approval of the plans and drawings referred to in subsection (4), a municipality may require the owner of the land to,*

*(a) provide to the satisfaction of and at no expense to the municipality any or all of the following:*

- 1. Subject to the provisions of subsections (8) and (9), widenings of highways that abut on the land.*
- 2. Subject to the Public Transportation and Highway Improvement Act, facilities to provide access to and from the land such as access ramps and curbing and traffic direction signs.*
- 3. Off-street vehicular loading and parking facilities, either covered or uncovered, access driveways, including driveways for emergency vehicles, and the surfacing of such areas and driveways.*
- 4. Walkways and walkway ramps, including the surfacing thereof, and all other means of pedestrian access.*
- 4.1 Facilities designed to have regard for accessibility for persons with disabilities.*
- 5. Facilities for the lighting, including floodlighting, of the land or of any buildings or structures thereon.*
- 6. Walls, fences, hedges, trees, shrubs or other groundcover or facilities for the landscaping of the lands or the protection of adjoining lands.*
- 7. Vaults, central storage and collection areas and other facilities and enclosures for the storage of garbage and other waste material.*
- 8. Easements conveyed to the municipality for the construction, maintenance or improvement of watercourses, ditches, land drainage works, sanitary sewage facilities and other public utilities of the municipality or local board thereof on the land.*
- 9. Grading or alteration in elevation or contour of the land and provision for the disposal of storm, surface and waste water from the land and from any buildings or structures thereon;*

The proposed development for the Subject Site has already provided a road widening to the municipality and will provide the additional features as found under Section 41 (7) of the Planning Act:

- facilities to provide access to and from the land such as curbing and traffic direction signs;
- off-street vehicular parking facilities;
- asphalt access driveways, including driveways for emergency vehicles;
- concrete walkways and walkway ramps;
- site lighting;
- an enclosed structure for the storage of garbage and other waste material;
- easements will be conveyed to the municipality for the construction, maintenance or improvement of land drainage works, sanitary sewage facilities and other public utilities of the municipality on the land; and
- grading of the site elevation and provision for the disposal of storm, surface and waste water from the land and from any buildings or structures thereon.



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## **Appendix D**

Review of Provincial Policy  
Statement (2014)



## Provincial Policy Statement (2014)

This Site Plan is consistent with the vision and ideas of the Provincial Policy Statement (PPS) as issued under Section 3 of the *Planning Act*. Section 1.1 of the PPS focuses on promoting efficient land use and development patterns that occur adjacent to built-up areas and which provide opportunities for intensification. Section 1.1.3.1 states:

*'Settlement Areas shall be the focus of growth and their vitality and regeneration shall be promoted.'*

Section 1.1.3.2 of the Provincial Policy Statement states:

*'Land use patterns within settlement areas shall be based on:*

- a) *densities and a mix of land uses which:*
  - 1. *efficiently use land and resources;*
  - 2. *are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for unjustified and/or uneconomical expansion;'*

Section 1.1.3.6 of the Provincial Policy Statement states:

*'New development taking place in designated growth area should occur adjacent to the existing built-up area and shall have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities'.*

The PPS promotes the development of areas where infrastructure is already in place. The PPS supports a range of housing types and Section 1.4.3 indicates that:

*"Planning authorities shall provide for an appropriate range of housing types and densities to meet projected requirements of current and future residents of the regional market area by:*

- a) *establishing and implementing minimum targets for the provision of housing which is affordable to low and moderate income households.....;*
- d) *promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of alternative transportation modes and public transit in areas where it exists or is to be developed; ..."*

Section 1.0 of the PPS supports building strong healthy communities through a number of strategies, are achieved by:



- a) Promoting efficient development and land use patterns which sustain the financial wellbeing of the Province and municipalities over the long term;
- b) Accommodating an appropriate range and mix of residential (including second units, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;
- c) Avoiding development and land use patterns which may cause environmental or public health and safety concerns;
- d) Avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;
- e) Promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;
- f) Improving accessibility for persons with disabilities and older persons by identifying, preventing and removing land use barriers which restrict their full participation in society;
- g) Ensuring that necessary infrastructure, electricity generation facilities and transmission and distribution systems, and public service facilities are or will be available to meet current and projected needs; and
- h) Promoting development and land use patterns that conserve biodiversity and consider the impacts of a changing climate.”

The proposed Jazz Lite residential development meets the vision of the PPS, as it:

- Provides for a density of residential development that is *appropriate for, and* supports the existing infrastructure capacity of the neighbourhood; and
- Promotes growth on vacant lands.

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## **Appendix E**

Review of City of Ottawa  
Official Plan

(Including OPA #196 and #150,  
#180 as they have been Council  
Approved)



## City of Ottawa Official Plan, 2003

The City of Ottawa Official Plan, adopted by City Council in May 2003, has been updated and amended numerous times by both Council and the Ontario Municipal Board. For the purposes of this planning rationale, the on-line consolidated version of the Official Plan, including Amendment #150 and #180, was used as reference along with OPA #196.

The Subject Site is located within the urban boundary of the City of Ottawa and is part of the settlement area for the City. Schedule 'B' of the Official Plan shows the site location in the south part of the City of Ottawa designated as a General Urban Area (refer to Figure 3) within the Urban Area.

Section 3.6.1 of the Official Plan provides a framework for the General Urban Area. The Plan states that:

*“The General Urban Area designation permits the development of a full range and choice of housing types to meet the needs of all ages, incomes and life circumstances, in combination with conveniently located employment, retail, service, cultural, leisure, entertainment and institutional uses. This will facilitate the development of complete and sustainable communities. A broad scale of uses is found within this designation, from ground-oriented single-purpose to multi-storey mixed-use; from corner store to shopping centre.*

*While the City is supportive of the establishment of a broad mix of uses in Ottawa's neighbourhoods, this is not meant to imply that all uses will be permitted everywhere within areas that are designated General Urban Area. The zoning by-law will continue to regulate the location, scale and type of land use in accordance with the provisions of this Plan. Within neighbourhoods, the zoning by-law will allow those uses that provide for the local, everyday needs of the residents, including shopping, schools, recreation and services. Uses that also serve wider parts of the city will be located at the edges of neighbourhoods on roads where the needs of these land uses (such as transit, car and truck access, and parking) can be more easily met and impacts controlled. Subject to the policies below, the City supports infill development and other intensification within the General Urban Area in a manner that enhances and complements the desirable characteristics and ensures the long-term vitality of the many existing communities that make up the city.”*

The proposed Site Plan was created with special consideration for the policies of the General Urban Area, Strategic Directions and Urban Design and Compatibility in the Official Plan and

based upon the required technical studies. The proposed Site Plan and current zoning are the proper reflection of these policies as refined in the supporting studies.

The proposed development is designated General Urban Area in Schedule B, which permits the development of a full range and choice of housing types to meet the needs of all ages, incomes and life circumstances. Within this area, the City encourages a broad scale of uses is found within this designation, from ground-oriented single-purpose buildings to mid-rise buildings that complement adjacent residential land uses, and are of a size and scale consistent nearby areas. The City manages this growth by ensuring that:

1. The use is compatible and complement surrounding land uses, and can be developed in accordance with Section 2.5.1 and Section 4.11;
2. The use supports intensification in the General Urban Area where it will complement the existing pattern and scale of development and planned function of the area;
3. The use is compatible with existing community character so that it enhances and builds upon desirable established patterns of built form and open spaces;
4. The use is contributing to the maintenance and achievement of a balance of housing types and tenures to provide a full range of housing for a variety of demographic profiles throughout the General Urban Area;
5. The use is intended to cluster with community-oriented uses, such as parks, pedestrian linkages, community centres or leisure facilities, in order to facilitate interaction among residents and contribute to a sense of community;
6. The use is situated to take advantage of pedestrian and cycling patterns;
7. The proposed size and scale is suitable with surrounding densities and uses.

The proposed residential development has been designed in accordance with Sections 2.5.1 and 4.11 and to a scale that is suitable with surrounding services and densities. The Subject Site is also located within vacant area of the greater residential community providing multimodal connectivity to the surrounding neighbourhood and is clustered with other community-oriented uses such as the two schools, parks and retirement facilities.

#### **4.2.1 Urban Design and Compatibility – Sections 2.5.1 and 4.11**

Sections 2.5.1 and 4.11 in the Official Plan outlines guiding policies to utilize when evaluating where and how land use should be permitted to develop. The proposed development has reviewed these sections and has been designed to meet and address the design objectives of these policies.

The proposed residential development has addressed the design objectives of Section 2.5.1 as follows:

Design Objective	Proposed Development
1. To enhance the sense of community by creating and maintaining places with their own distinct identity	The proposed development provides an opportunity to bring a higher residential form to the existing residential, commercial and institutional neighbourhood.
2. To define quality public and private spaces through development	The proposed development has been designed creating quality exterior space for each dwelling unit and their anticipated needs.
3. To create places that are safe, accessible and are easy to get to, and move through	The proposed development has been designed to City of Ottawa standards including adequate lighting to provide easy access onto the Subject Site from roads, sidewalks, and parking areas.
4. To ensure that new development respects the character of existing areas	The surrounding area is primarily residential with two existing commercial plazas within the immediate area. Therefore, the density and architectural design will respect and complement the existing uses and architecture of the area.
5. To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.	The proposed development provides a new form of residential density to the area bringing a new choice for residents who wish to live within this community
6. To understand and respect natural processes and features in development design	The proposed development has been designed in a manner which utilizes the natural topography of the Subject Site to manage drainage.
7. To maximize energy-efficiency and promote sustainable design to reduce the	The proposed development has been designed to provide as much landscaping as

resource consumption, energy use, and carbon footprint of the built environment	possible, while utilizing energy efficient building construction.
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The proposed development has addressed the Policies of Section 4.11 as follows:

<b>Policies</b>	<b>Proposed Development</b>
Traffic: Roads should adequately serve the development, with sufficient capacity to accommodate the anticipated traffic generated. Generally, development that has the potential to generate significant amounts of vehicular traffic should be located on arterial or major collector roadways so as to minimize the potential for traffic infiltration on minor collector roadways and local streets.	A Traffic Impact Study was completed by IBI Group which determined that the existing network can support the proposed development.
Vehicular Access: The location and orientation of vehicle access and egress should address matters such as the impact of noise, headlight glare and loss of privacy on development adjacent or immediately opposite. Vehicular access and egress for development that has the potential to generate a significant amount of vehicular traffic should be oriented on streets other than local streets, wherever the opportunity exists, considering traffic safety and other transportation objectives of this Plan.	The Subject Site will be accessible from Bank Street and Longworth Ave to ensure that traffic flows efficiently.
Parking Requirements: The development should have adequate on-site parking to minimize the potential for spillover parking on adjacent areas. A range of parking forms, including surface, decked, and underground, should be considered taking in account the area context and character. Opportunities to reduce parking requirements and promote increased usage of walking, cycling and transit will be pursued, where appropriate,	A total of 142 surface parking spaces are proposed for this development which exceeds the zoning by-law minimum parking requirements for the proposed dwelling units and visitor spaces.

particularly in the vicinity of transit stations or major transit stops in accordance with the provisions of Section 4.3.	
Outdoor Amenity Areas: The development should respect the privacy of outdoor amenity areas of adjacent residential units and minimize any undesirable impacts through the siting and design of the buildings and the use of screening, lighting, landscaping or other mitigative design measures.	The proposed exterior amenity space being provided for each dwelling unit has been located internal to the site in order to help respect the privacy of outdoor amenity areas of adjacent residential units as well as for the units onsite.
Loading Areas, Service Areas, and Outdoor Storage: The operational characteristics and visual appearance of loading facilities, service areas (including garbage), parking and areas for the outdoor storage of goods or materials should be mitigated using a variety of methods (e.g., location, containment, screening, berms, and/or landscaping). These uses and activities should be located away from residences where possible.	The uses of the proposed buildings do not require loading spaces per the Zoning By-law.
Lighting: The potential for light spill over or glare from any lighting source onto adjacent light-sensitive areas should be avoided or mitigated.	The lighting standards will adhere to the City's standards with no anticipated impacts onto adjacent properties.
Noise and Air Quality: The development should be located and designed to minimize the potential for significant adverse effects on adjacent sensitive uses related to noise, odours, and other emissions.	It is anticipated that any potential noise or air quality impacts are mitigated through the design of the Subject Site.
Sunlight: The development should minimize shadowing on adjacent properties, to the extent practicable, particularly on outdoor amenity areas, through the siting of buildings or other design measures.	The proposed maximum height of approximately 11 m metres as well as the ample distance provided between buildings should positively address this policy.



<p>Microclimate: The development should be designed to minimize adverse effects related to wind, snow drifting, and temperature on adjacent properties.</p>	<p>The proposed density and spacing on site should positively address this policy and mitigate most wind or snow drifting concerns.</p>
<p>Supporting Neighbourhood Services: The development should contribute to or be adequately served by existing or proposed services and amenities such as health facilities, schools, parks and leisure areas. Where the proposed development itself is to contribute such services and amenities, they should be of a scale appropriate to the needs and character of the area.</p>	<p>The proposed development will be adequately served by existing surrounding services and amenities such as the Vimy Ridge Public School and the proposed school block for the French Catholic School Board.</p>

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## **Appendix F**

Review of the City of Ottawa Zoning  
By-law (2008-250)



## **City of Ottawa Zoning By-law 2008-250**

On June 27<sup>th</sup>, 2018 the Subject Site was rezoned to GM – General Mixed Use in the City of Ottawa Comprehensive Zoning By-law 2008-250 by By-law #221-2018, as shown on Figure 5.

The purpose of the GM zone is to:

- “(1) allow residential, commercial and institutional uses, or mixed use development in the General Urban Area and in the Upper Town, Lowertown and Sandy Hill West Character Areas of the Central Area designations of the Official Plan;*
- (2) limit commercial uses to individual occupancies or in groupings in well defined areas such that they do not affect the development of the designated Traditional and Arterial Mainstreets as viable mixed-use areas;*
- (3) permit uses that are often large and serve or draw from broader areas than the surrounding community and which may generate traffic, noise or other impacts provided the anticipated impacts are adequately mitigated or otherwise addressed; and*
- (4) impose development standards that will ensure that the uses are compatible and complement surrounding land uses.”*

The uses permitted in the GM zone and zoning standards are as follows:

### **GM – General Mixed Use Zone (Sec. 187-188)**

187. In the GM Zone, the following residential uses are permitted subject to the provisions of subsections 187(3), (4) and (5):

- a maximum of ten guest bedrooms in a bed and breakfast
- apartment dwelling, low rise
- apartment dwelling, mid rise
- bed and breakfast
- dwelling unit
- group home
- planned unit development
- retirement home
- retirement home, converted
- rooming house

- stacked dwelling
- townhouse dwelling

**TABLE 187 – GM ZONE PROVISIONS**

<b>I ZONING MECHANISMS</b>			<b>II PROVISIONS</b>
(a) Minimum lot area			No minimum
(b) Minimum lot width			No minimum
(c) Minimum front yard and corner side yard setbacks			3 m
(d) Minimum interior side yard setbacks	(i) for a non-residential use building or a mixed residential / non-residential use building, from that portion of a lot line abutting a residential zone		5 m
	(ii) for a residential use building	1. for a building equal or lower than 11 metres in height	1.2 m
		2. for a building higher than 11 metres in height	3 m
	(iii) other cases		No minimum
(e) Minimum rear yard setbacks	(i) abutting a street		3 m
	(ii) for a non-residential use building or a mixed residential / non-residential use building, from that portion of a rear lot line abutting a residential zone		7.5 m

	(iii) for a residential use building	7.5 m
	(iv) other cases	No minimum
(f) Maximum building height		18 m
(g) Maximum floor space index		2
(h) Minimum width of landscaped area	(i) abutting a street	3 m
	(ii) abutting a residential or institutional zone	3 m
	(iii) other cases	No minimum
(i) minimum width of landscaped area around a parking lot		<i>see Section 110 – Landscaping Provisions for Parking Lots</i>
(4)	Storage must be completely enclosed within a building.	
(5)	For other applicable provisions, see Part 2 – General Provisions, Part 3 – Special Use provisions, and Part 4 – Parking and Loading Provisions. (By-law 2016-249)	

The proposed Site Plan demonstrates the conformity with the zoning standards of the GM zone Section 187-188 and all other relevant General and Parking provisions.





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