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**2740 Cedarview Road and 4190, 4200,
4210, and 4236 Fallowfield Road,
Ottawa, Ontario**

**Planning Rationale & Integrated
Environmental Review Statement**

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**2740 CEDARVIEW ROAD AND
4190, 4200, 4210 AND 4236 FALLOWFIELD ROAD
OTTAWA, ONTARIO**

**PLANNING RATIONALE & INTEGRATED
ENVIRONMENTAL REVIEW STATEMENT (IERS)
IN SUPPORT OF APPLICATIONS FOR
SECONDARY PLAN AMENDMENT, ZONING BY-LAW AMENDMENT,
AND SITE PLAN CONTROL**

Prepared For:

Phoenix Harbour Fallowfield Road Inc. (c/o DCR Phoenix)

Prepared By:

NOVATECH
Suite 200, 240 Michael Cowpland Drive
Ottawa, Ontario
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July 30, 2018
Novatech File: 113209
Ref: R-2018-081
Revision 02: August 30, 2019

August 30, 2019

City of Ottawa
Planning & Growth Management Department
110 Laurier Avenue West
Ottawa, ON, K1P 1J1

Attention: Sean Moore, Planner III

Dear: Mr. Moore

**Reference: Fallowfield and Cedarview Condos
Planning Rationale for Secondary Plan Amendment, Zoning By-law
Amendment, and Site Plan Control Applications
2740 Cedarview Road; and 4190, 4200, 4210 and 4236 Fallowfield Road
Our File No.: 113209
City File No.: D07-12-18-0177, D02-02-18-0072, & D01-01-18-0008**

Novatech has been retained by Phoenix Harbour Fallowfield Road Inc. (the owner) to prepare this revised Planning Rationale in support of applications for *Official Plan (Secondary Plan) Amendment, Zoning By-law Amendment, and Site Plan Control* for its property at the corner of Fallowfield Road and Cedarview Road, immediately west of Barrhaven. This property is currently known as civic addresses 2740 Cedarview Road; and 4190, 4200, 4210 and 4236 Fallowfield Road and is herein called the 'Subject Site'.

The site plan for the Subject Site has been revised since the first application submission with consideration of City staff's planning comments in the latest revision. Phoenix Harbour Fallowfield Road Inc. is proposing to develop one hundred ninety-four (194) units as part of a planned unit development on the Subject Site. The development will consist of five (5) "Apartment Buildings" (Apartment Dwelling, Low Rise) containing a total of 90 units with underground parking and an elevator, three (3) "Back-to-Back Terrace Homes" (Stacked Dwelling) containing a total of 48 units with separate entrances and surface parking, four (4) "Terrace Flats" (Stacked Dwelling) containing a total of 48 units, and one (1) "Terrace Homes" (Stacked Dwelling) containing a total of 8 units.

The Subject Site will be accessed from a newly created private street with two entrances – one off Fallowfield Road and one off Cedarview Road. Resident parking is located above and below grade, and visitor parking is located above grade.

Yours truly,

NOVATECH



Robert Tran, M.PL.
Planner, Planning & Development

CONTENTS

1.0	INTRODUCTION	1
1.1	Site Description and Surrounding Uses	1
1.2	Proposed Development Details.....	2
1.3	Applications and Supporting Studies.....	4
	1.3.1 Proposed Official Plan (Secondary Plan) Amendment.....	4
	1.3.2 Proposed Zoning By-law Amendment	4
	Proposed Site Plan Control.....	4
	1.3.3 Supporting Plans and Studies	4
2.0	POLICY AND REGULATORY FRAMEWORK AND PLANNING RATIONALE.....	5
2.1	Provincial Policy Statement.....	5
2.2	Official Plan	7
	2.2.1 Integrated Environmental Review Statement.....	15
2.3	Nepean South Areas 9 and 10 Secondary Plan	17
2.4	Zoning By-law 2008-250	20
	2.4.1 Proposed Exception to R4Z Zone.....	20
3.0	CONCLUSION.....	21

FIGURES

Figure 1: Location of Subject Site and Surrounding Land Use Designations.....	2
Figure 2: Excerpt from the Site Plan.....	3
Figure 3: Amenity Area and Calculations.....	3
Figure 4: Official Plan Designations.....	8
Figure 5: Front Elevation of Apartments (Buildings 3, 4, 5, 6, and 7).....	13
Figure 6: Front Elevation of 'Terrace Homes' (Stacked Townhouse Buildings 9, 10, and 11)	14
Figure 7: Front elevation of 'Terra Flats' (Stacked Townhouse Buildings 1, 2, 12, and 13)	14
Figure 8: South Nepean Areas 9 and 10 Secondary Plan Designations.....	18
Figure 9: Proposed Area for Rezoning.....	20

APPENDICES

Appendix A: Cedarview Road and Fallowfield Road Site Plan Rev 12, Dated August 29, 2019,
Prepared by M. David Blakely Architect Inc.

1.0 INTRODUCTION

This revised Planning Rationale will demonstrate that the proposed development will be consistent with the *Provincial Policy Statement* (2014), generally conform to the *City of Ottawa's Official Plan* and *Nepean South Areas 9 & 10 Secondary Plan*, and generally comply to the provisions of *Zoning By-law 2008-250* as well as respects the applicable urban design guidelines found in the *Official Plan*.

Novatech has been retained by Phoenix Harbour Fallowfield Road Inc. (the owner) to prepare this revised Planning Rationale in support of applications for *Official Plan (Secondary Plan) Amendment*, *Zoning By-law Amendment*, and *Site Plan Control* for its property at the corner of Fallowfield Road and Cedarview Road, immediately west of Barrhaven. This property is currently known as civic addresses 2740 Cedarview Road; and 4190, 4200, 4210 and 4236 Fallowfield Road and is herein called the 'Subject Site'.

One hundred ninety-four (194) units are proposed for the Subject Site. The development will consist of five (5) "Apartment Buildings" (Apartment Dwelling, Low Rise) containing a total of 90 units with underground parking and an elevator, three (3) "Back to Back Terrace Homes" (Stacked Dwelling) containing a total of 48 units with separate entrances and surface parking, four (4) "Terrace Flats" (Stacked Dwelling) containing a total of 48 units, and one (1) "Terrace Homes" (Stacked Dwelling) containing a total of 8 units as shown on **Figure 2**.

All units will be accessed from walkways and a newly created private street that bisects the site from east to west. The private street will have two accesses - one off Fallowfield Road and one off Cedarview Road. Two spacious communal amenity areas proposed, which are located west on the Subject Site near the school park and in between Buildings 8 and 9. Residents will also have access to private amenity space in the form of balconies. A total of 274 parking spaces are provided whereas 272 are required as per the zoning by-law provisions. Each of the five apartment buildings contains 14 resident parking spaces below grade for a total of 70 spaces below grade. All other required parking, including required visitor parking, is provided above grade.

To permit the proposed development, an amendment to the *South Nepean Secondary Plan Areas 9 and 10* is required to: re-designate the Subject Site from *Institutional/Office* to *Residential*; request relief from the permitted maximum building height; and add specific policy regarding the high density residential node for the Subject Site towards the eastern edge. An amendment to *Zoning By-law 2008-250* is required to rezone the Subject Site from *Development Reserve (DR)*. It is suggested the zoning should be amended to *Residential Fourth Density Subzone Z (R4Z)* with an exception to increase the gross floor areas of the accessory buildings and reduced interior side yard setback for one of the principle dwelling as discussed further in Section 2.4.1 of this report.

1.1 Site Description and Surrounding Uses

The Subject Site has an area of 24,352.8 m² and is shown in **Figure 1**. Currently on site are three detached dwellings on septic – three to the northeast corner of the site and one associated with an old farm at the northwest of corner which has been demolished since the first submission. Several accessory buildings, including a large, unused barn still remain on the old farm site but will be removed prior to construction.

The Subject Site is bound to the north by Fallowfield Road, to the east by Cedarview Road, to the south by Cedarview Middle School and to the west by existing detached dwellings around a cul-de-sac. North of Fallowfield Road are estate-like lots in the City's Rural Area. East of Cedarview Road are smaller detached dwellings in the City's Urban Area (Barrhaven West). The Subject Site is immediately southwest ('kitty-corner') to the greenbelt.



Figure 1: Location of Subject Site and Surrounding Land Use Designations.

The Subject Property is identified on GeoOttawa by the following civic addresses and are identified legally by the associated legal descriptions:

- 4190 Fallowfield Road: PT LT 20 CON 4 RF BEING PART 1 ON PLAN 4R-13732
- 4200 Fallowfield Road: PT LT 20 CON 4 RF AS IN CR429834
- 4210 Fallowfield Road: PCL 1-2 SEC 4D-82 PTS 1 & 5, 4D-82 S/T 725941
- 4236 Fallowfield Road: PART OF LOT 20, CONCESSION 4, RIDEAU FRONT, NEPEAN, PARTS 2 AND 4 ON PLAN 4R7681, SAVE AND EXCEPT PARTS 1 AND 2 ON PLAN 4R20148; OTTAWA
- 2740 Cedarview Road: PT LT 20 CON 4 RF AS IN CR711578 NEPEAN

The subject site is designated and zoned in the following planning documents as:

- | | |
|--|---------------------------|
| • <i>Official Plan, Schedule B:</i> | General Urban Area |
| • <i>South Nepean Area 9 Secondary Plan:</i> | Institutional/Office Area |
| • <i>Zoning By-law 2008-250:</i> | Development Reserve (DR) |

For more information on the planning policy and regulation context see Section 2.0 this report.

1.2 Proposed Development Details

One hundred ninety-four (194) units are proposed for the Subject Site. The development will consist of five (5) "Apartment Buildings" (Apartment Dwelling, Low Rise) containing a total of 90 units with underground parking and an elevator, three (3) "Back to Back Terrace Homes" (Stacked Dwelling) containing a total of 48 units with separate entrances and surface parking, four (4) "Terrace Flats" (Stacked Dwelling) containing a total of 48 units, and one (1) "Terrace Homes" (Stacked Dwelling) containing a total of 8 units as shown on **Figure 2**. All units will be accessed

from walkways and a private street that bisects the site from east to west. The private street will have two accesses – one off Fallowfield Road and one off Cedarview Road.

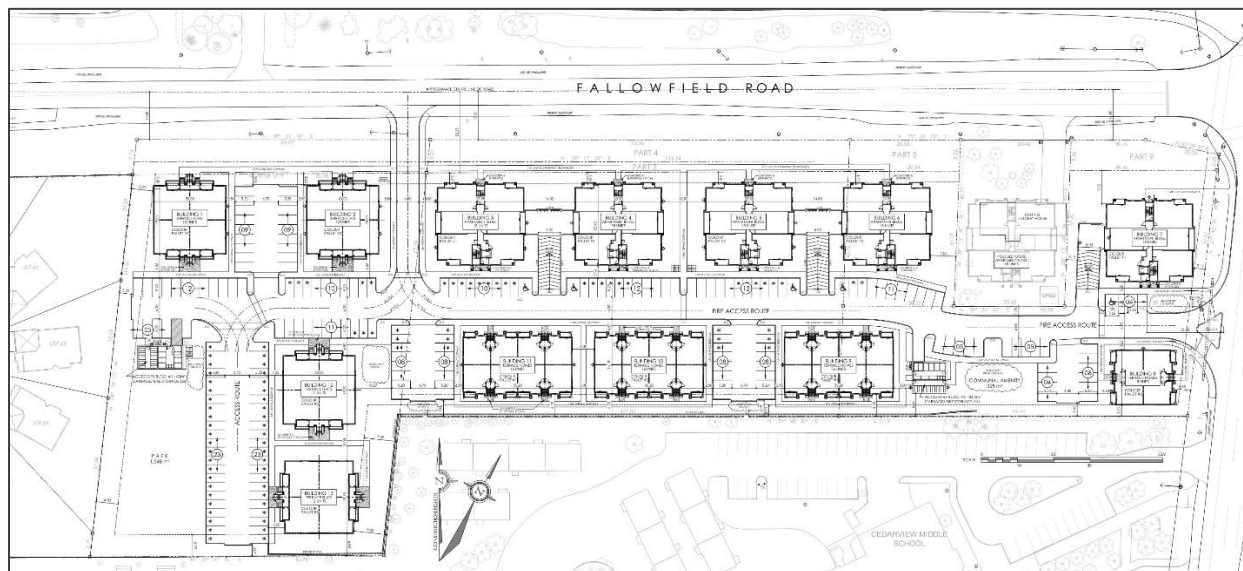


Figure 2: Excerpt from the Site Plan Rev 12, dated August 29, 2019, prepared by M. David Blakely Architect Inc.

The site plan layout features two spacious communal amenity spaces, one 1,548 m² which abuts the school park and the existing residential units to the southeast and the other 325 m² in between Buildings 8 and 9 as shown on **Figure 3**. Each unit will also have access to individual private amenity space in the form of balconies.

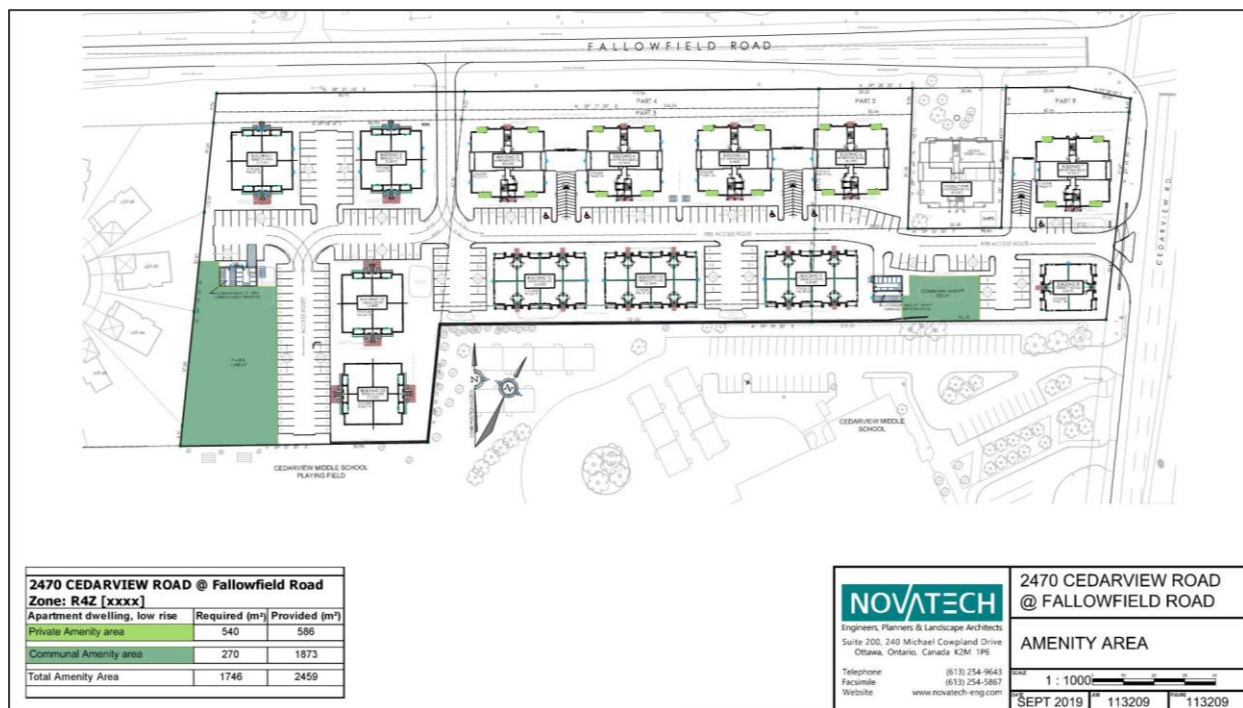


Figure 3: Amenity Area and Calculations.

1.3 Applications and Supporting Studies

1.3.1 Proposed Official Plan (Secondary Plan) Amendment

The *Nepean South Area 9 and 10 Secondary Plan* requires an amendment to enable the Subject Site to be designated *Residential* to allow for residential uses as proposed in the report. The Subject Site is currently designated *Institutional/Institutional Office Area*. The amendment will also request that specific policy be added to Section 2.2.1 of the Secondary Plan for the Subject Site to permit the proposed maximum building height of 12.1 metres for the ground oriented units.

Details of applicable policies and proposed policy changes are found in Section 2.3 of this report.

1.3.2 Proposed Zoning By-law Amendment

Residential uses are only permitted on the Subject Site if subject to a *Zoning By-law* amendment. The Subject Site is currently zoned *Development Reserve (DR)* and it is suggested that lands be rezoned to R4Z[xxxx] subject to review and modification by City Staff. The following exception to the R4Z zone is suggested:

- To permit accessory buildings a cumulative floor area of more than 55 m² measured from the exterior of walls of the building, whereas the *Zoning By-law* provides a maximum of 55 m² as measured from the exterior walls of the building [*Zoning By-law 2008-250* Table 55(6)]; and
- To permit an interior side yard setback of 3 m for Building 8 whereas the *Zoning By-law* provides a minimum interior side yard setback of 6 m (*Zoning By-law 2008-250* Table 162A, Column X, Subzone Z, for Planned Unit Development).

Proposed Site Plan Control

The Site Plan Control application provides City staff sufficient detail with regards to the visual aspects of the site and its impacts on the private and public realms. Further design details of the proposed development are provided in Section 2.2 below.

1.3.3 Supporting Plans and Studies

The following plans and studies are provided as part of the three applications described above:

Plans:

- **Site Plan**, prepared by M. David Blakely Architect Inc., dated January 2017, revision #12 dated August 29, 2019
- **Landscape Plans (L1 and L2)**, prepared by James B. Lennox and Associates Inc., dated April 2018, revised dated September 2019;
- **Tree Conservation Plan (TCR)**, prepared by James B. Lennox and Associates Inc., dated April 2018, revised dated September 2019;
- **Grade Control and Drainage Plans (GR1, GR2 and GR3)**, prepared by Novatech, revision #2 dated August 29, 2019;
- **Removals & Erosion and Sediment Control Plan (ESC)**, prepared by Novatech, revision #2 dated August 29, 2019;
- **Plan and Profile Sanitary Extension Plans (PR1, PR2, PR3 and PR4)**, prepared by Novatech, revision #2 dated August 29, 2019;
- **Sanitary Drainage Area Plans (SAN1 and SAN2)**, prepared by Novatech, revision #2 dated August 29, 2019;

- **Stormwater Management Plans (STM1 and STM2)**, prepared by Novatech, revision #2 dated August 29, 2019;
- **Servicing Plans (GP1 and GP2)**, prepared by Novatech, revision #2 dated August 29, 2019;
- **Area Location Plan (ALP)**, prepared by Novatech, revision #2 dated August 29, 2019; and
- **Architectural Elevations (A1-A14)**, prepared by M David Blakely Architect Inc., revision #2 dated May 2018, dated August 28, 2019

Reports:

- **Planning Rationale**, prepared by Novatech dated July 30, 2018, revision 2, dated August 29, 2019;
- **Traffic Impact Assessment Report (TIA)**, prepared by Novatech dated June 20, 2018 revised August 2019;
- **Site Servicing & Stormwater Brief**, prepared by Novatech, dated July 26, 2018, revised August 29, 2019;
- **Detailed Noise Study**, prepared by Novatech, dated July 26, 2018, revised August 29, 2019; and
- **Scoped Environmental Impact Statement – Initial Report (EIS)**, prepared by Kilgour and Associates, dated June 11, 2018, revised August 15, 2019

2.0 POLICY AND REGULATORY FRAMEWORK AND PLANNING RATIONALE

The following section will demonstrate how the proposed *Official Plan* (Secondary Plan) Amendment, *Zoning By-law* Amendment, and Site Plan applications are consistent with relevant Provincial planning policies and generally conforms to City of Ottawa planning policies as well as generally complies with the *Zoning By-law* regulations.

2.1 Provincial Policy Statement

The *2014 Provincial Policy Statement (PPS)* provides policy direction on land use planning and development matters of provincial interest, as set out in section 2 of the *Planning Act*. All decisions affecting planning matters are required to “be consistent with” relevant policies of the PPS.

Section 1.1 of the PPS speaks to managing and directing land use to achieve efficient and resilient development and land use land patterns.

PPS: Settlement Areas

Policy 1.1.3.1 states, “Settlement areas shall be the focus of growth and development, and their vitality and regeneration shall be promoted.”

- The proposed development is located on lands designated by the City of Ottawa as settlement area (*General Urban Area*).

Policy 1.1.3.4 states, “Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.”

- The proposed development consists of denser stacked, and apartment dwellings. These dwellings will be supported by a variety of amenities within walking distance such as the neighbourhood park, a school, open spaces, commercial services, and the growing Strandherd business area.

Policy 1.1.3.5 states, “Planning authorities shall establish and implement minimum targets for intensification and redevelopment within built-up areas, based on local conditions...”

- The proposed development achieves the minimum targets on Schedule B for Areas 9 and 10 of the South Nepean Secondary Plan. The Subject Site will contribute 194 units in Area 1, which targets a minimum of 490 units. Approximately three hundred and thirty-six (336) units are already built within Area 1 as shown on Schedule B with no other lands readily available for development in this area.

Policy 1.1.3.6 states, “New development taking place in designated growth areas should occur adjacent to the existing built-up area and shall have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities.”

- The proposed development is located within the City of Ottawa’s *General Urban Area* and is occurring adjacent to existing built-up lands.

PPS: Housing

Policy 1.4.3 states,

Planning authorities shall provide for an appropriate range and mix of housing types and densities to meet projected requirements of current and future residents of the regional market area by:

...

b. permitting and facilitating:

- 1. all forms of housing required to meet the social, health and well-being requirements of current and future residents, including special needs requirements; and*
- 2. all forms of residential intensification, including second units, and redevelopment in accordance with policy 1.1.3.3;*

c. directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;

d. promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed; and

e. establishing development standards for residential intensification, redevelopment and new residential development which minimize the cost of housing and facilitate compact form, while maintaining appropriate levels of public health and safety.

- The proposed development offers stacked townhouses and low-rise apartment buildings, which are generally not available in the Secondary Plan Area and nearby communities. Some apartment-style dwellings are available at Strandherd Drive and Kennevale Drive, and a retirement community is south of the railway on Strandherd Drive.
- The proposed density is higher than the *typical* net densities for high density development described in the *Secondary Plan*, however the location of the proposed density offers future residents access to the adjacent school and park, the growing Strandherd business area, transit options, and convenient access to Highway 416.
- The proposed development will provide new residential housing that is compact in form and can minimize the costs of housing.

PPS: Sewage, Water and Stormwater

Section 1.6.6 of the PPS guides development with regards to sewage, water and stormwater. Policy 1.6.6.2 states, “Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas.”

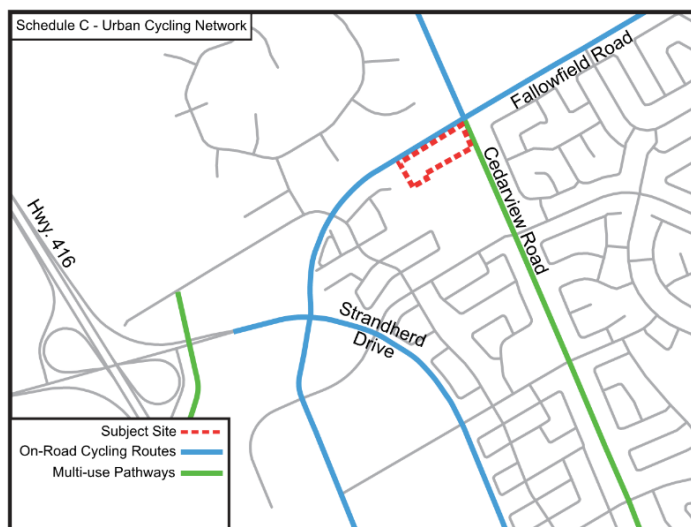
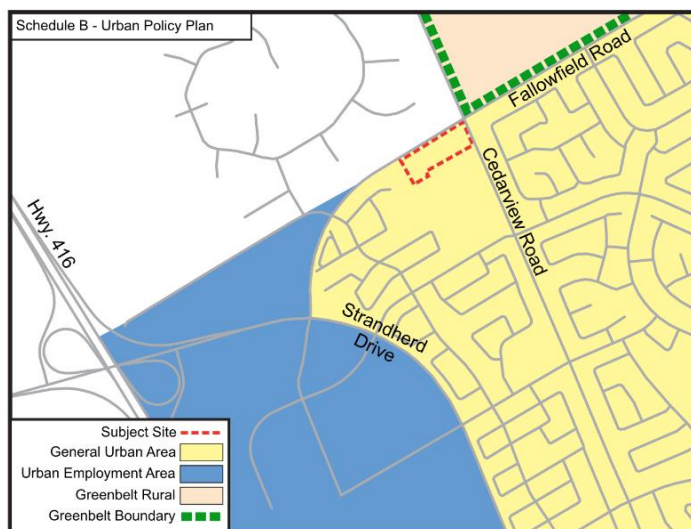
- The proposed development will use exclusively municipal sewage, water and stormwater services.

2.2 Official Plan

Relevant sections and policies of the Official Plan are listed below. The planning rationale responding to each policy is identified by the bulleted text.

OP Section 3.6.1 - Land Use Designations

The Subject Site is designated ‘General Urban Area’ on Schedule B of the City of Ottawa Official Plan. Schedule E of the Official Plan identifies Fallowfield Road as an ‘Arterial’ and Cedarview Road as a ‘Major Collector’. Official Plan Designations are shown on **Figure 4**.



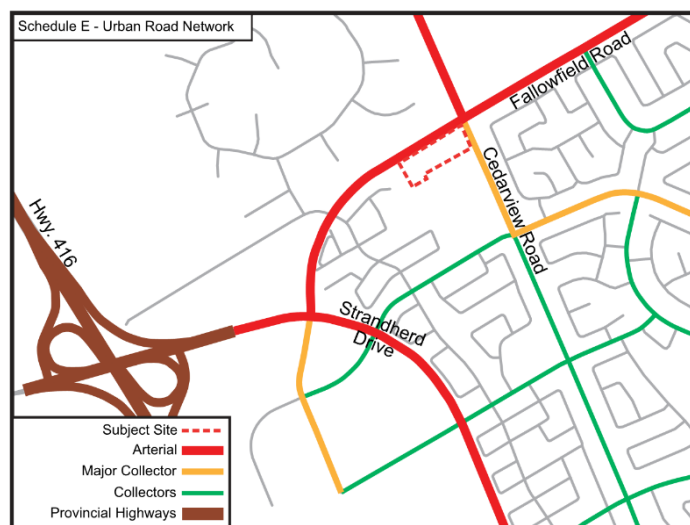


Figure 4: Official Plan Designations.

Section 3.6.1 of the *Official Plan* states that the General Urban Area “permits the development of a full range and choice of housing types to meet the needs of all ages, incomes and life circumstances, in combination with conveniently located employment, retail, service, cultural, leisure, entertainment and institutional uses.” Applicable policies under Section 3.6.1 are listed below:

Policy 1 states, “General Urban Area areas are designated on Schedule B. The General Urban Area designation permits all many types and densities of housing, as well as employment, retail uses, service, industrial, cultural, leisure, greenspace, entertainment and institutional uses”.

- The proposed stacked townhouse and low-rise apartment dwellings are permitted at the density proposed.

Policy 2 states, “The evaluation of development applications, studies, other plans and public works undertaken by the City in the General Urban Area will be in accordance with Section 2.5.1 and Section 4.11”.

- These sections are addressed further in this report.

Policy 3 states, “Building height in the General Urban Area will continue to be predominantly Low-Rise. Within this range, changes in building form, height and density will be evaluated based upon compatibility with the existing context and the planned function of the area. Secondary plans or zoning that currently permit building heights greater than four Storeys will remain in effect”.

- The maximum height determined by the zoning by-law provisions for low-rise apartment dwellings and stacked dwellings is 15 m. All the proposed buildings are designed to be 2.5 or 3.5 storeys measuring less than 12.7 m in height as shown on the submitted elevation plans. However, the *South Nepean Secondary Plan Areas 9 and 10* determines that a maximum height for “ground-oriented development” shall be 10.7 m. An amendment will be required to the *Secondary Plan* to remove the maximum height requirements.

OP Section 4.1: Site-Specific Policies and Secondary Policy Plans

Policy 1 states, “Secondary plans, for villages and urban areas, and site-specific policies found in Volume 2 provide more detailed policy directions for specific areas or neighbourhoods”.

- Policies of the secondary plan will be addressed in Section 2.3 of this report.

OP Section 4.3: Walking, Cycling, Transit, Roads, and Parking Lots

Policy 1 states, “The road network in new plans of subdivision will provide the opportunity for direct transit routes through the community and for all buildings to be within 400 metres walking distance of a transit stop”.

- The proposed development is within 150 m of a transit stop on Cedarview that offers OC Transpo routes 170 and 270.

Policy 5 states, “The City will require a transportation impact assessment report, which may be a community transportation study, transportation impact study, or transportation brief to be submitted where the City determines that the development may have an impact on the transportation network in the surrounding area”.

- A Transportation Impact Assessment dated June 20, 2018 and revised August 2019 have been prepared by Novatech for the applications noted in this report. A Multi-use path (MUP) is located on the north side of Fallowfield Road between Cedarview Road and O’Keefe Court.

OP Section 4.4.1: Servicing in Public Service Areas

Policy 1 states, “When reviewing development applications, the City will require an assessment of the adequacy of public services to support the development”.

- A Servicing Design Brief dated July 26, 2018 and revised August 9, 2019 by Novatech to assess the serviceability of the subject site. This report also discusses how run-off from the proposed development will be managed. (See Section 2.2.1 of this report for more information)

OP Section 4.6.1 – Heritage Buildings and Areas

No heritage buildings or areas are located on or adjacent to the Subject Site.

OP Section 4.6.2: Archaeological Resources

Policy 1 states, “Where development is proposed on land where archaeological potential exists, as identified on the City of Ottawa map *Areas of Archaeological Potential* the City will require an archaeological resource assessment to be conducted by an archaeologist licensed under the Ontario Heritage Act...”.

- Stage One and Two Archaeological Assessments have been completed by Adams Heritage and were submitted following the submission of the applications. The conclusion from the Stage One assessment resulted in the recommendation for a Stage Two assessment to be completed. The Stage Two Archaeological Assessment was completed and provided to the City for review with no further comments from staff.

OP Section 4.7.2 – Protection of Vegetation Cover

Policy 1 states, “...applications for subdivision, condominium and site plan approval, affecting vegetation cover on site, will be supported by a Tree Conservation Report and a Landscape Plan.”

- A Tree Conservation Report (TCR) revision dated September 2019 and a Landscape Plan revision dated September 2019 are both prepared by James B Lennox & Associates Inc. These plans are included with this application. The TCR and Landscape Plan respond to the

remaining policies of Section 4.7.2 of the Official Plan. (See Section 2.2.1 of this report for more information.)

OP Section 4.7.4 – Protection of Endangered and Threatened Species

Policy 2 states, “... Significant habitat of endangered and threatened species will be identified by: ... An Environmental Impact Statement in areas where there is potential for significant habitat to exist...”.

- An EIS dated May 17, 2018 and revised August 15, 2019 by Kilgour Associates studied the endangered and threatened species and is being resubmitted as part of this application. (See Section 2.2.1 of this report for more information.)

OP Section 4.7.6 – Stormwater Management

Policy 1 states, “A stormwater site management plan will be required to support subdivision and site-plan applications”.

- A Stormwater Management report, combined with the Servicing Report dated July 26, 2018 and revised August 29, 2019 by Novatech, is being submitted as part of this revised application and will address any relevant policies of the Official Plan and other City of Ottawa engineering requirements. (See Section 2.2.1 of this report for more information.)

OP Section 4.8.7 – Environmental Noise Control

Policy 3 states, “Development proposals for new noise sensitive land uses will require a noise feasibility study and/or detailed noise study in the following locations: ... (b) 100 metres from the right-of-way of: (i) an existing or proposed Arterial, Collector or Major Collector Road identified on Schedules E and F...”.

- A detailed noise study for traffic noise from both Fallowfield (Arterial) and Cedarview (Major Collector) dated July 26, 2018 and revised August 15, 2019 by Novatech forms part of this revised application. The report recommends a combination of measures to achieve the noise requirements, including select warning clauses, architectural wall sections, and protection of an amenity area. (See Section 2.2.1 of this report for more information.)

OP Section 4.10 - Greenspace Requirements

Policy 1 states, “As a condition of development or redevelopment, the City will acquire land for park purposes through the provisions of the *Planning Act*, in a way that best meets park and recreation needs of the community”.

- Cash-in-lieu of parkland will be required.

Section 2.5.1 outlines the *Official Plan's Design Objectives* and speaks to achieving development that “enhances the established community” and “coexists with existing development”. The following is a review of the seven stated design objectives and summary of how the proposal is consistent with these objectives:

1. *To enhance the sense of community by creating and maintaining places with their own distinct identity.*
 - The identity of the area could be classified as low-rise residential with complementary institutional uses and easy access to business uses. The proposed low-rise development

adds to this characterisation and enhances it by bringing more residents to use the mix of uses nearby.

2. To define quality public and private spaces through development.

- The proposed buildings face all directions and abut the public streets and private street. The private amenity spaces are adjacent to the school site and will provide residents a peaceful area to use. The buildings provide passive surveillance along the private street.

3. To create places that are safe, accessible and are easy to get to, and move through.

- The proposed buildings have large windows, balconies, and entrances that look over the public and private streets, parking and amenity areas, as shown on **Figure 5**, **Figure 6**, and **Figure 7**. The common areas will be afforded passive surveillance. Site lighting and landscaping will be designed to ensure sightlines to and from pedestrian walkways, parking areas, building entrances and garbage areas.
- A series of pedestrian sidewalks are located on the site to facilitate safe movement to and from, and within, the site as shown on revised site plan in **Appendix A**.

4. To ensure that new development respects the character of existing areas.

- The proposed three-storey apartment buildings are similar in height to the abutting school and are only one storey higher than the large detached dwellings to the west. Similar rooflines, building articulation, and building material types will complement the detached dwellings, which helps to integrate the proposed buildings into the community's existing character.

5. To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.

- The proposed development for the Subject Site is not intended to evolve in the near or longer-range future. It is consistent with the Plans for the area save for the prescriptive maximum height limit described in section 2.2.1 of the *Secondary Plan*. The proposed buildings offer two housing choices – apartments and stacked townhouses.

6. To understand and respect natural processes and features in development design.

- The Subject Site has been informed by the recommendations of those studies examining the natural landscape of the site as listed in the supporting plans and studies.

7. To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.

- Higher density forms of development allow for more efficient use of land. The proposed development will have a net residential density of 79.8 units per net hectare (the private street and amenity areas have not been excluded from this calculation).

Section 4.11: Urban Design and Compatible Development speaks to neighbourhood or site-scale issues such as noise, light spillover, parking and access, shadowing, and microclimate on proposed and existing development. Policies of Section 4.11 and how the proposed development satisfies these policies are as follows.

1. Traffic.

- The TIA was prepared by Novatech dated June 2018 and revised August 2019. Some of the findings from the report are: the proposed development is not anticipated to have a

significant impact on the existing vehicular operations along Cedarview Road south of Fallowfield Road; the site generated traffic is not anticipated to have a significant impact on the signalized intersections within the study area.

2. Vehicular Access.

- As part of the proposed development, a newly created private street will be developed to provide access to the site. Two accesses and egresses are provided – one on Fallowfield Road and the other on Cedarview Road. The access on Cedarview Road will be limited to a 'right-in and right-out' as to minimize conflicts and eliminate the conflicts with northbound queues as per the City's first round of engineering and planning comments received.

3. Parking Requirements.

- The proposed development will feature a total of two hundred seventy-four (274) parking spaces are provided, whereas two hundred seventy-four (272) are required as per the parking by-law provisions. Each of the five apartment buildings contains 14 resident parking spaces below grade for a total of 70 spaces below grade at the Subject Site. All other required parking is provided above grade. Accessible parking is provided throughout the Subject Site.

4. Outdoor Amenity Areas.

- The site plan layout features two spacious amenity areas to serve the future residents of the proposed development. Future residents will also have access to private amenity spaces in the form of balconies which will be appropriately setback and buffered from adjacent properties to mitigate any concerns regarding noise or loss of privacy.

5. Loading Areas, Service Areas, and Outdoor Storage.

- The proposed development will feature two accessory buildings which will be used for the purposes of garbage and recycling collection as well as bicycle storage. The location of these accessory buildings have been appropriately setback from adjacent land uses to ensure concerns regarding noise or odour are mitigated.

6. Lighting.

- The proposed development will not generate a spillover or glare from any lighting sources onto adjacent light-sensitive areas. A lighting certificate to verify the City's standards of 0.5 foot candles at the property line will be provided at a later date.

7. Noise and Air Quality.

- The proposed development will not generate adverse effects on adjacent sensitive uses related to noise, odours, and other emissions.

8. Sunlight.

- The heights of the proposed buildings will not generate shadowing on streetscapes or adjacent land uses. The proposed three-storey apartment buildings are similar in height to the abutting school and are only one storey higher than the large detached dwellings to the west.

9. Microclimate.

- The proposed development will not have any effects related to wind, snow drifting, and temperature on adjacent properties.

10. Supporting Neighbouring Services.

- The proposed development is served by local community amenities including the Cedarview School, Cedarview Alliance Church, and Strandherd Business Park.

Building Design

Buildings on site were designed by M. David Blakely Architect Inc. A perspective of some of the apartment buildings is shown on **Figure 5**, **Figure 6**, and **Figure 7**. The following discussion was prepared by the architect:

The dominant exterior material is a sand-coloured brick that provides a strong building base which rises to form square arches to identify the building entrance and frame the first-floor balconies. The second-floor balconies sit on the brick arches with balcony guards, located between square columns, which support the balcony roofs. The result is that the balconies are built into the building façade and provide a semi-private separation from the private street.

The darker walnut colour siding provides a complimentary contrast to the brick, and a backdrop to the square arches. The low slope roof has large overhangs, providing a strong horizontal shadow line, and a comfortable building cap. Windows and patio door openings are large, with divider detailing complimentary, and in proportion, to the elevations. Window frames, metal-work, and light fixtures are all chestnut bronze colour, providing a unifying detail to the building.

The proposed buildings are all located adjacent to the private street. Buildings 1 – 7 front directly onto Fallowfield Road and provide an animated elevation, as do Buildings 7 and 8 on Cedarview Road; sidewalks on Fallowfield and Cedarview extend to the sidewalk along the private street and the entrances of the buildings. Many balconies and living area windows overlook the private street and some overlook the amenity space while others look over the public roads. All views from within the buildings provide passive surveillance on and around the site.



Figure 5: Front Elevation of Apartments (Buildings 3, 4, 5, 6, and 7).



Figure 6: Front Elevation of 'Terrace Homes' (Stacked Townhouse Buildings 8, 9, 10, and 11).



Figure 7: Front elevation of 'Terra Flats' (Stacked Townhouse Buildings 1, 2, 12, and 13).

Massing and Scale

The proposed buildings will have a height of 2.5 to 3.5 storeys and will be set back from the private street at least 3 m. Existing development to the south is buffered by a public school, school site and church property, which both approximate the proposed building height for the buildings on site. Dwellings to the west are approximately one storey less than that of the proposed buildings.

Outdoor Amenity Areas

Spacious communal amenity areas are located to the west of Buildings 12 and 13 and between Buildings 8 and 9. They are designed and landscaped for a range of uses and activities for future residents of the proposed development. Balconies and patios provide private outdoor amenity area.

2.2.1 Integrated Environmental Review Statement

Recommendations from the required studies are listed below. They have informed the design of the Subject Site in terms of grading, drainage, site services, fire protection, transportation, building structure and material requirements, and any mitigative measure needed. The site as proposed meets all regulatory requirements described in each plan and study. No significant barriers to development have been identified.

Servicing Design Brief

A Servicing Design Brief dated July 26, 2018, revised August 29, 2019 was completed by Novatech, with the following conclusions:

1. The proposed storm system will control post-development flow to pre-development conditions. All runoff volume from the 100-year storm event is stored on site. The Gregory Casey Stormwater Management Facility provides an enhanced level of water quality control.
2. The proposed sanitary sewer conforms to City design criteria and provides a gravity outlet for the development site. The existing septic systems will be decommissioned in accordance with Schedule 10 from the Ottawa Septic Office. Flows do not exceed the available capacity of the Tartan Pump Station. There is sufficient capacity in the downstream sanitary sewers to accommodate the ultimate flows outletting to the future South Nepean Collector.
3. Connection to the existing watermain in Fallowfield and Cedarview will provide municipal water service to the development. The existing wells will be abandoned in accordance with O. Reg. 903 s.21.
4. There is adequate fire protection to the proposed development, in accordance with the Fire Underwriter's Survey.
5. The proposed infrastructure (sanitary, storm and water) complies with City of Ottawa design standards.

Geotechnical Study

A Geotechnical Study dated June 4, 2018 was completed by EXP. Some conclusions are:

1. There are no grade-raise restrictions at the site.
2. The geotechnical conditions at the site are suitable for construction of the proposed residences
3. The lowest level floors of the proposed buildings may be constructed as slabs-on-grade.

Archeological Assessment Stages 1 and 2

A Stage 1 Archaeological Assessment dated April 16, 2018 was prepared by Adams Heritage. The result of the Stage 1 Archaeological Assessment was that a Stage 2 assessment was required. The subsequent Stage 2 Archaeological Assessment dated September 17, 2018 was completed by Adams Heritage following further seasonal field work in the fall of 2018. The recommendation/conclusion of the Stage 2 Archaeological Assessment was that no further archaeological assessment of the property was required.

Traffic Impact Assessment (TIA)

A TIA dated June 6, 2018 and revised August 2019 was completed by Novatech. Some of the conclusions and recommendations from the report include:

1. On-site pedestrian facilities will be provided between the main building entrances and the parking lot;
2. A temporary asphalt pathway will be provided within the Fallowfield Road right-of-way to connect the northern entrances to buildings 1-6 to other on-site pathways. As the 4192 Fallowfield Road property is not part of this application, the extension of the east-west pathway within the Fallowfield Road right-of-way to the Fallowfield Road/Cedarview Road intersection is not possible.
3. OC Transpo bus stops #5452 and #1728 are located in the southeast and northwest corners of the signalized access to Cedarview Middle School (2760 Cedarview Road), at a walking distance of approximately 150m from the proposed development.
4. The width and location of the Fallowfield Road access meets the minimum requirements of the City's ZBL and Private Approach By-law.
5. The width of the Cedarview Road access at the property line is driven by the pork chop island. As such, a waiver to the Private Approach By-law is requested.

Noise Study for traffic noise on Fallowfield and Cedarview

A Noise Study dated July 18, 2018 and revised August 29, 2019 was completed by Novatech. To meet the requirements for compliance with the City of Ottawa Environmental Noise Control Guidelines and the MOE Environmental Noise Guideline the following measures are required:

1. No barriers are proposed within the development.
2. Warning Clauses: Warning clauses are to be placed on title and in the purchase and sale agreements.

Scoped Environmental Impact Statement (EIS; for barn swallows)

An Initial EIS report dated June 11, 2018 and revised on August 15, 2019 was prepared by Kilgour & Associates Ltd. The revised EIS was prepared following the May-July 2018 season required for work relating endangered and threatened species. The EIS concludes with the following:

1. Possible Barn Swallow nests were observed in the large barn at the southwest corner of the site. Following further site investigations in the summer of 2018, it was confirmed that the potential Barn Swallow nests were used by Eastern Phoebes with no Barn Swallows observed flying on or near the site.
2. No SAR or SAR habitats are currently located on, or adjacent to the property, or are likely to occur there in the foreseeable future. Therefore, no impacts to SAR or SAR habitats are anticipated.
3. No natural features occur on site or within 120 m of the site. Therefore, no specific additional mitigations are required.
4. Culverting of the site channel will require a permit to alter a waterway from the RVCA. The application for this permit will be supported by the HDFA, which will be completed in the summer of 2018.

Tree Conservation Report (TCR) Plan

A TCR plan was completed by James B. Lennox and Associates Ltd. revision dated September 2019. Some trees along Fallowfield will be retained as shown on the TCR Plan. During construction it may be determined possible to retain the larger oak trees at the northwest of the site. However, most trees on the Subject Site must be removed prior to development. New trees will be planted as shown on the Landscape Plans (L1 and L2) revision dated September 2019.

Phase I Environmental Site Assessment (ESA)

A Phase I ESA dated March 29, 2018 was prepared by EXP. A Phase II was determined to be required for the area in front of 4190 Fallowfield Road. The Phase II ESA dated April 10, 2018 was prepared by EXP with conclusions and recommendations from the ESA include:

1. Based on the results of the Phase One ESA completed at 4190, 4200, 4210, 4236 Fallowfield Road and 2740 Cedarview Road in Ottawa, a Phase Two ESA is required to assess the soil and groundwater conditions.

Phase II ESA

A Phase II ESA dated April 10, 2018 was prepared by EXP. Conclusions from the report include:

1. No additional work is recommended to address the APECs identified in the previous Phase One ESA.
2. If the wells are no longer needed, they should be decommissioned in accordance with Ontario Regulation 903.

2.3 Nepean South Areas 9 and 10 Secondary Plan

The Subject Site is presently designated as *Institutional Office Area* in the *Secondary Plan*. An amendment to the Secondary Plan is required to enable residential uses on the Subject Site and to remove the height maximum of 10.7 m for ground oriented residential development in Section 2.2.1.

The first part of the amendment is to enable residential uses on the Subject Site. *Section 2.2.3 Institutional Areas/Institutional Office Area* guides the direction for development in the *Institutional/Office Area* designation. This Section is comprised of four paragraphs. The first two regard institutional uses and the latter two regard office uses. City Staff members determined through an email in April 2018 that only the second two paragraphs in *Section 2.2.3* apply to the Subject Site. Therefore, an amendment is requested to change the designation to *Residential* from *Institutional/Office Area*, which is described in *Section 2.2.1 Residential Areas* as well as enable an amendment to the policies that dictate residential density patterns in the area.

The amendment requests that the proposed development be permitted to deviate from *Section 3.1 Principles of Design and Development* which notes that “residential densities and housing forms should make a transition from the highest densities at the western edge, through medium density, to low density on the eastern edge”. The proposed amendment would request that the Subject Site be identified as a ‘high density’ residential node towards the eastern edge within the policies of the *Secondary Plan*. Presently, existing residential built form towards the western edge consists primarily of single detached homes and townhomes zoned at low to medium densities towards Strandherd Drive. With exception to the future retirement home, City File #D07-12-18-0196, zoned R5A H(18) proposed at the corner of Strandherd Drive and Maravista Drive by Claridge Homes, the existing residential built form within this area does not achieve the desired ‘high density at the western edge’ as set out in the policies *Section 3.1* of the *Secondary Plan*.

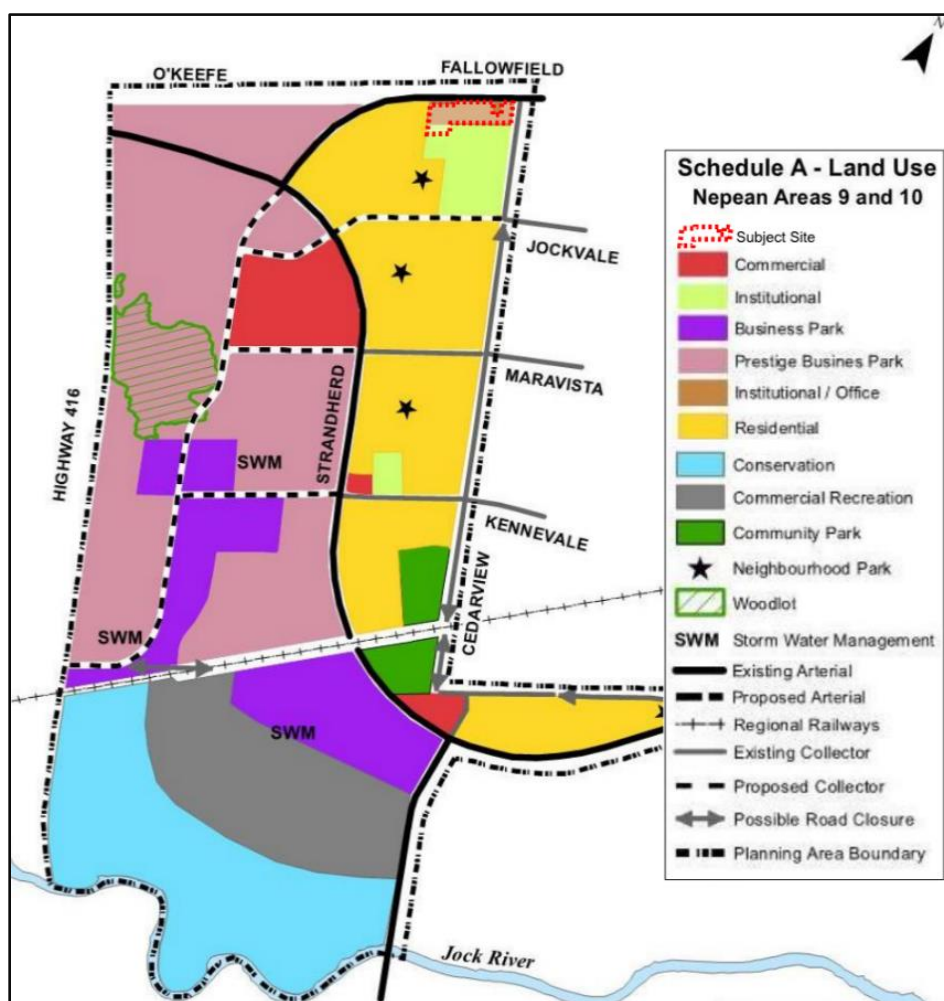


Figure 8: South Nepean Areas 9 and 10 Secondary Plan Designations.

Institutional/Office Area uses are more appropriately sited in the western half of the *South Nepean Areas 9 and 10 Secondary Plan* area as seen in **Figure 8**. This large area designated *Prestige Business Park, Business Park* and *Commercial* would be desirable for additional *Institutional/Office Area* uses by providing for a centralized area of compatible land uses. All the uses listed in the *Institutional Office Area* designation can be accommodated in the nearby business park and commercial locations west of Strandherd Drive. The amendment of the Secondary Plan from *Institutional/Institutional Office Area* to *Residential* will enable the target units listed in the *Secondary Plan Section 2.2.1 Residential* to be achieved. The proposed development will contribute 194 units in Area 1, which targets a minimum of 490 units. Approximately three hundred and thirty-six (336) units are already built within Area 1 as shown on Schedule B with no other lands readily available for development in this area.

Further, the second part of the *Secondary Plan* amendment is to amend the maximum building height of 10.7 m in Section 2.2.1 for ground-oriented residential development. The amendment requests that specific policy be added to the *Secondary Plan* for the Subject Site to permit the maximum building height of 12.1 m for ground oriented units. Nevertheless, the maximum building heights of the proposed buildings comply with the zoning by-law provisions for low-rise apartment dwellings and stacked dwellings which is 15 m. All the proposed buildings are designed to be 2.5 or 3.5 storeys measuring less than 12.7 m in height as shown on the submitted elevation plans.

Moreover, the *City of Ottawa Official Plan* Section 3 states, “A land-use designation in an Official Plan is implemented through a range of more detailed land-use zones in a Zoning By-law. While the Zoning By-law is more detailed, it must conform to the policies in the Official Plan”. The suggested zoning conforms to the *Official Plan* as the R4 zone permits building heights of four storeys or less, which is considered ‘low rise residential’ built form in the *Official Plan* Section 3.6.1 Policy 3. A purpose of the R4 zone is to “allow a wide mix of residential building forms ranging from detached to low rise apartment dwellings, in some cases limited to four units, and in no case more than four storeys, in areas designated as General Urban Area in the Official Plan” (*Zoning By-law 2008-250, R4 - Residential Fourth Density Zone, Purpose of the Zone*). The suggested zoning also conforms to the *Secondary Plan* as described above.

The *South Nepean Area Plan* is derived from the Former City of Nepean documents that have been carried over with the 2001 amalgamation of the City of Ottawa and surrounding areas. The *Zoning By-law 2008-250* provides more up-to-date direction for built form as it was created to implement the intents of the *City of Ottawa Official Plan*. The height limit found in the Secondary Plan should be removed for the Plan to be consistent with the policies of the *Official Plan* and the *Zoning By-law*.

Further, it is understood from discussions with City senior management that the City of Ottawa is currently working towards phasing out Secondary Plans such as *South Nepean Areas 9 and 10 Secondary Plan* due to the dated nature of policies that do not have any relevancy to the present built environment. In doing so, this will eliminate the overlapping and duplication of zoning by-law provisions with policies of Secondary Plans.

2.4 Zoning By-law 2008-250

The Subject Site is currently zoned as *DR-Development Reserve* under City of Ottawa *Zoning By-law 2008-250*. The purpose of the *DR-Development Reserve Zone* is to recognize lands intended for future urban development in areas designated as *General Urban Area* and *Developing Community* in the *Official Plan*. The DR zone is essentially a placeholder to prevent development before studies have been completed and approved. Such studies have been completed and are being revised, where required with this application to rezone the site to support the proposed urban land uses.

The applicant is proposing to rezone the entirety of the property with a suggested R4Z[xxxx] as shown on **Figure 9**. This zone provides the flexibility needed to achieve the type of neighbourhood design expected in new developments. A variety of building forms and building designs are permitted, such as a mixture of detached, semi-detached, townhouses, stacked townhouses and low-rise apartments. The provisions of Section 131 – *Planned Unit Developments* of the Zoning By-law will apply. The proposed exemption to the zone is described below.

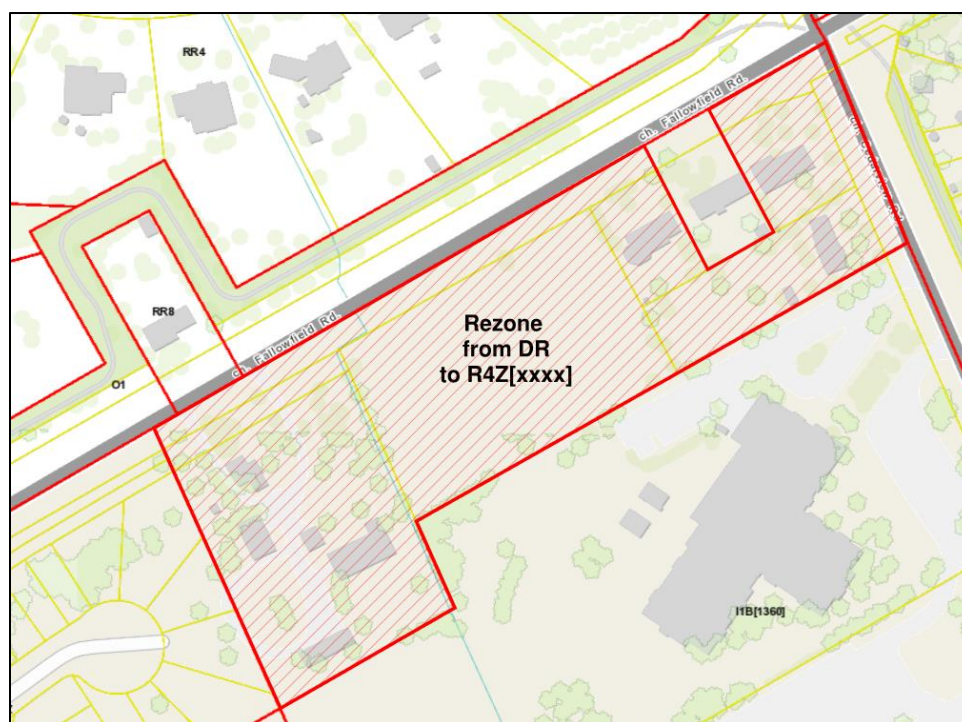


Figure 9: Proposed Area for Rezoning.

2.4.1 Proposed Exception to R4Z Zone

The following is proposed as exceptions to the R4Z zone:

- To permit accessory buildings a cumulative floor area of more than 55 m² measured from the exterior of walls of the building, whereas the *Zoning By-law* provides a maximum of 55 m² as measured from the exterior walls of the building [*Zoning By-law 2008-250* Table 55(6)]; and
- To permit an interior side yard setback of 3 m for one of the principle dwellings (Building 8) whereas the *Zoning By-law* provides a minimum interior side yard setback of 6 m (*Zoning By-law 2008-250* Table 162A, Column X, Subzone Z, for Planned Unit Development).

The proposed increases in accessory building area are required to ensure that the two accessory buildings will be adequate to accommodate the garbage and recycling storage needs of the 194 proposed dwelling units and to provide protected and secure bicycle parking in numbers that meet the bicycle parking provision in Section 111 (Table 111A)(b) of the *Zoning By-law*. Accommodating several smaller buildings on site would create visual clutter and potentially provide management confusion. The two proposed accessory buildings at the proposed footprint areas will allow for a more efficient and centralized waste management collection for operators servicing the proposed development.

The reduction to the interior side yard for Building 8 is to allow for a more efficient layout given the odd shape of the site. The reduction approximates that which is currently on site between the school site and the detached dwelling at 2740 Cedarview Road. Views from Cedarview down the interior side yard are blocked by existing trees on the school site that will remain in place. Trees align the majority of the interior lot line, which will buffer views of the proposed buildings from the school site. Further, the Subject Site does not abut any private amenity space nor residential development.

3.0 CONCLUSION

The proposed applications are consistent with the *Provincial Policy Statement*, generally conforms to the *Official Plan* and *Secondary Plan*, and generally complies with the proposed site-specific zoning.

It is Novatech's professional opinion that the proposed development is considered good planning. The proposed development is an appropriate and desirable addition to the Barrhaven area.

Yours truly,

NOVATECH

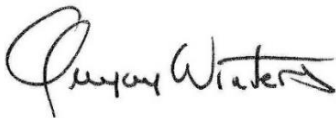
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Senior Project Manager, Planning & Development

Appendix A
Cedarview Road and Fallowfield Road
Site Plan REV 12, Dated August 29, 2019
Prepared by M. David Blakely Architect Inc.

